



Rock Creek Park Multi-Use Trail Project Project Booklet

December 2016

Presented to:



Presented by:



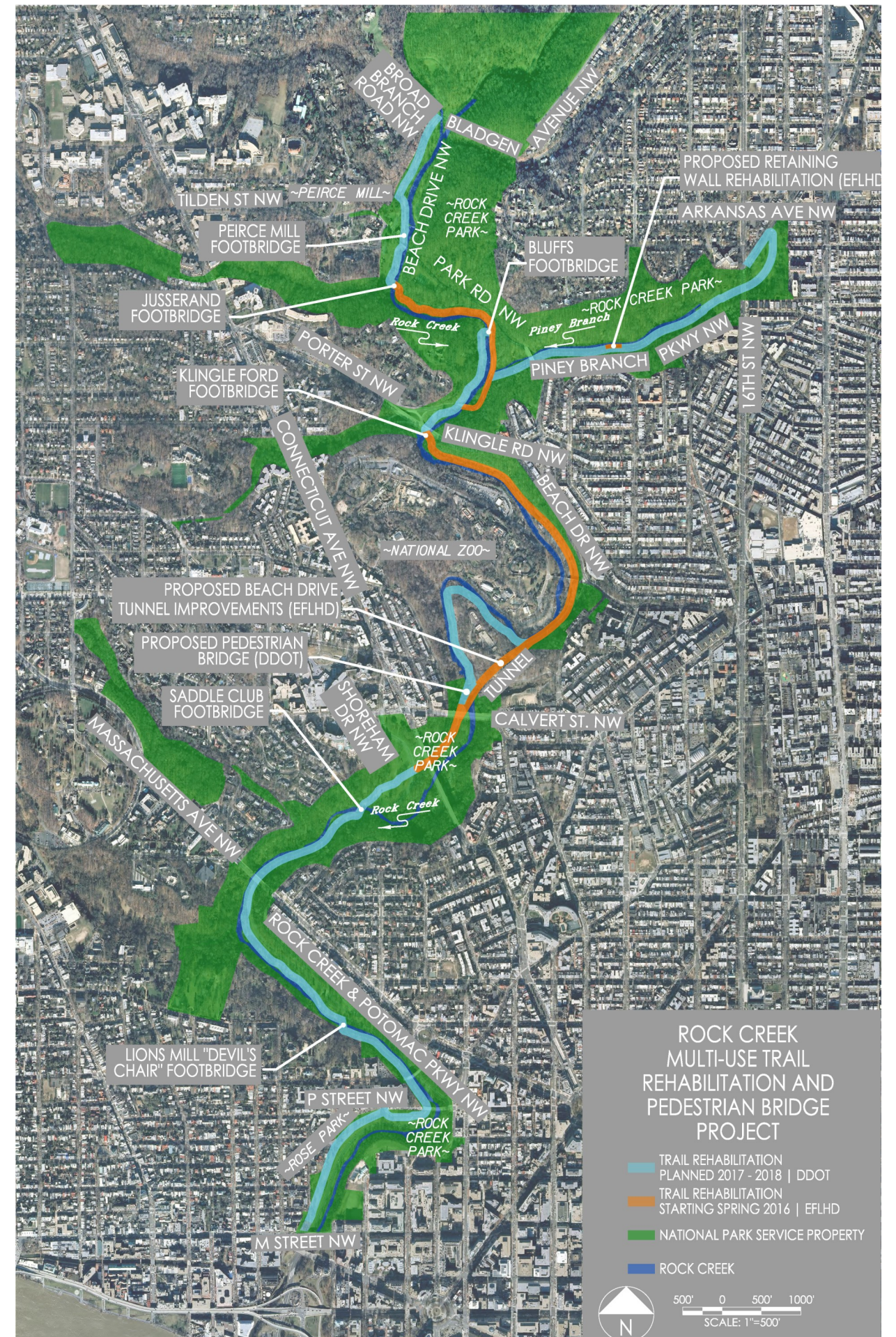
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Project Background

The Rock Creek Park Multi-Use Trail and Pedestrian Bridge Project was initially conceived in the early 2000's. Based on conceptual improvements developed in a planning study and because the project is on federal lands, it was determined that an environmental assessment was necessary prior to moving forward with design and construction. The District of Columbia Department of Transportation (DDOT) and the National Park Service (NPS) began preparing the Rock Creek Park Multi-Use Trail Rehabilitation Environmental Assessment (EA) in 2010, and it was issued for public review and comment in November 2011. The EA recommended Alternative 3: Trail Resurfacing and Widening. The EA recommended action also included a new pedestrian bridge crossing over Rock Creek, located at the south end of the Beach Drive tunnel.

From 2012 to 2013, DDOT and NPS also developed 30% design plans to support the EA document. In September 2013, the 30% design was finalized and the Finding of No Significant Impact (FONSI) determination was issued in June 2014. In 2015, the FHWA - Eastern Federal Lands Highway Division (EFLHD) agreed to incorporate part of the scope of this project into its Beach Drive NW Reconstruction project (PRA-ROCR, 10(5)), which began construction in September 2016. This design task is the final step toward implementation and construction of the remaining improvements.



Proposed Pedestrian Bridge

Site Context

The proposed pedestrian bridge site is located in Washington, DC within Rock Creek Park (NPS). The site is characterized by abruptly-rising, hilly, forested terrain to the north and east. A sprawling, sparsely wooded floodplain lies to the south and west. Beyond the floodplain, the terrain also rises abruptly into the city. Rock Creek is broad and shallow, flowing through the site in a southeasterly direction. The forest stands in the area are composed of tulip poplar, red maple, sycamore, white oak, box elder, beech, ash, and locust.

Park roadway, trail, bridge, and tunnel infrastructure bound the south and east sides of the site. The Duke Ellington Bridge overshadows the site from the south, rising high above Rock Creek to the elevation of the city above, carrying Calvert Street NW traffic over the park.

Beach Drive NW crosses Rock Creek on an existing two-lane bridge and passes through an 800-ft long tunnel before continuing to the north. Traffic volume on Beach Drive NW is approximately 23,000 vehicles per day. The existing roadway bridge was constructed in 1966. This bridge is not currently on the National Register of Historic Places but is eligible for consideration.

At this time, there are no plans to reconstruct the existing roadway bridge. However, the bridge and tunnel are currently undergoing rehabilitation improvements which began construction in September 2016.

The Rock Creek Park Multi-Use Trail passes through the site but is constricted by narrow walkways along the existing roadway bridge and tunnel.

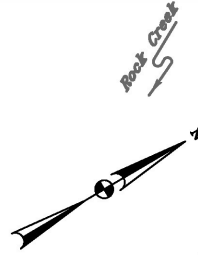
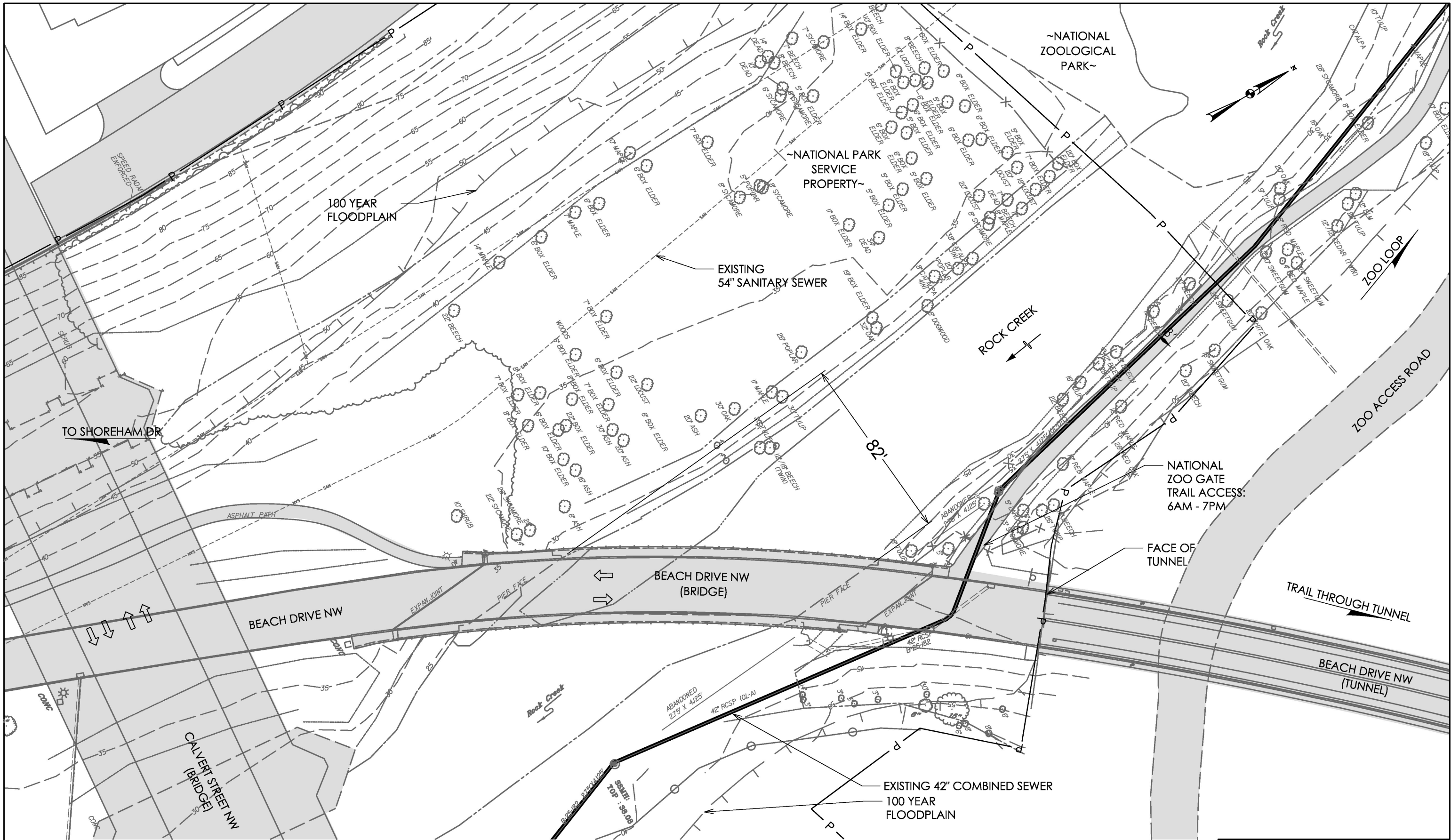
The National Zoo's property is located to the northwest. An existing trail through the zoo property provides passage for the multi-use trail and is a welcome alternative to the narrow tunnel walkway.



"The dominant consideration, never to be subordinated to any other purpose in dealing with Rock Creek Park, is the permanent preservation of its wonderful natural beauty, and the making of that beauty accessible to people without spoiling the scenery in the process."

-Frederick Law Olmsted

ROCK CREEK PARK
A report by OLMSTED BROTHERS
December 1918
FOREWORD



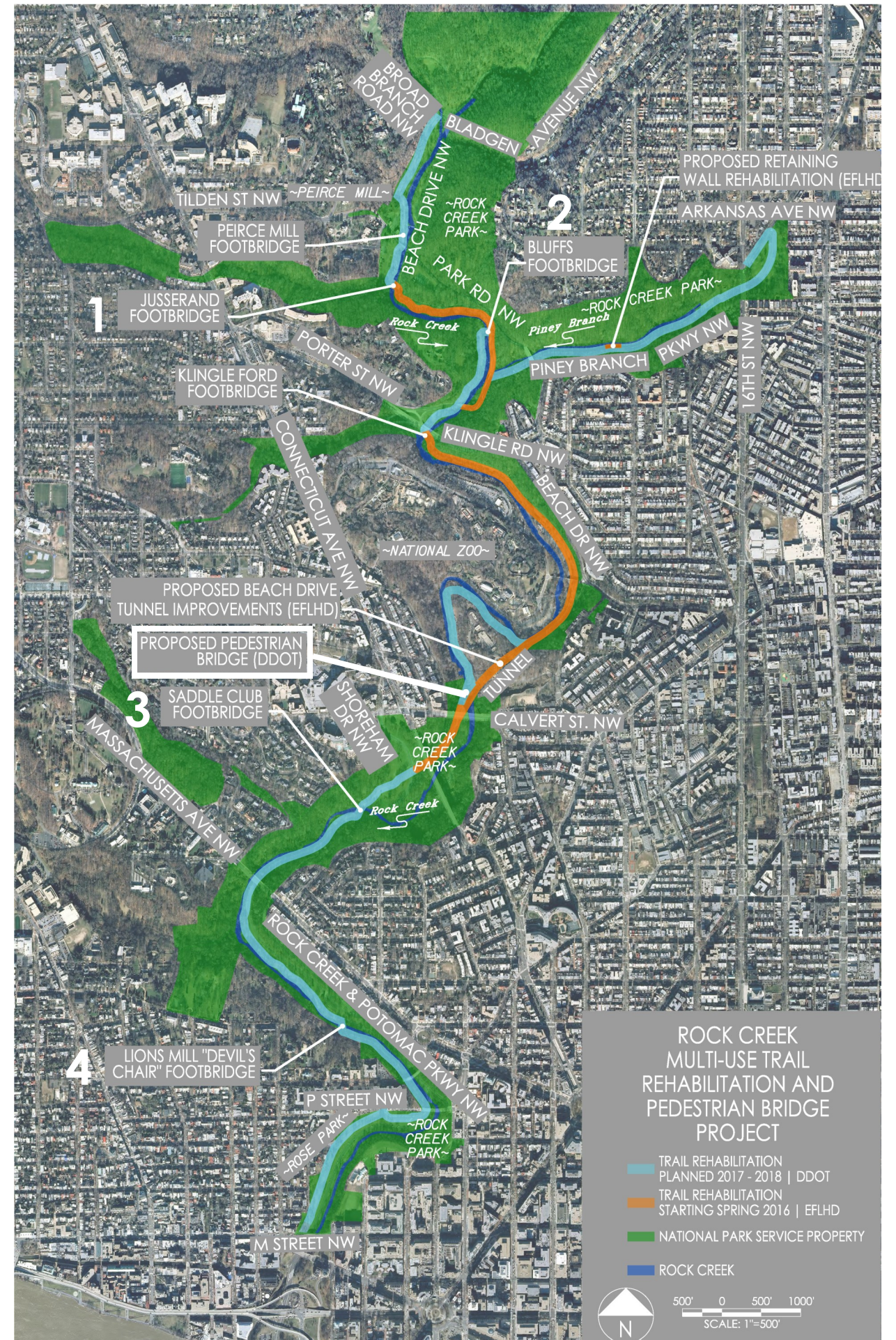
- LEGEND**
- P — NPS PARK BOUNDARY
 - EXISTING 42" COMBINED SEWER
 - ▭ EXISTING PAVEMENT



DATE: 10-2016	SCALE: 1" = 20'
EXISTING SITE PLAN	

Footbridges in the Park

There are a number of existing footbridges within the project area that were constructed in the 1930's. These bridges are cast-in-place concrete with cambered profiles and post & rail fall protection. Stonework is typically a random ashlar pattern with a rough exposed face using quarried stone materials such as granite, schist, and gneiss. Additional footbridges constructed of timber are located at Klinge Ford and Peirce Mill.



Proposed Pedestrian Bridge Overview

The purpose of the proposed pedestrian bridge is to provide safe passage for trail users across Rock Creek. Currently, two-way trail traffic — bicycles and pedestrians — cross along the west side of the Beach Drive NW bridge sidewalk, which is only 3-ft wide. The proposed bridge will be 10-ft wide.

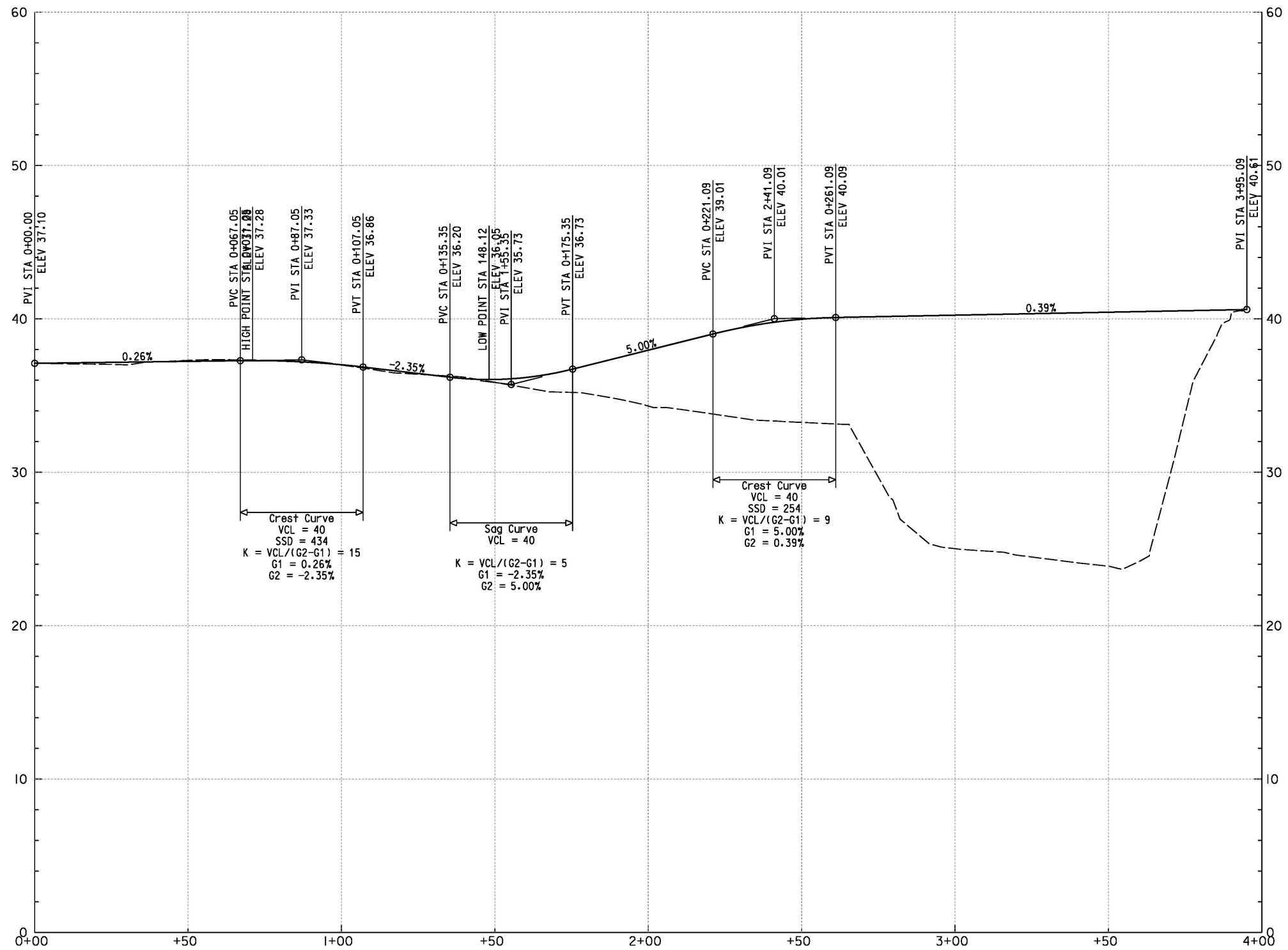
During the development of the Environmental Assessment, which included a 30% design effort, various alignment, bridge type, and material alternatives were considered. An alternative considered that would have widened the existing roadway bridge was not progressed due to technical challenges with the existing bridge design and condition. Additional alignment and bridge type alternatives were developed to the immediate west of the roadway bridge. The EA/FONSI preferred alternative was developed to match the alignment, hydraulic opening, and style of the existing roadway structure.

Based on recent meeting discussions with NPS, DDOT, CFA, and SHPO, the EA/FONSI preferred alternative was modified. The modified, recommended alignment is set on a tangent with a south-westerly bearing, angling away from the roadway bridge and tunnel at approximately 12 degrees. For functional purposes, the new bridge will align closely with the tunnel walkway. However, the proposed alignment provides enough separation so that the bridge can be designed with its own unique character. The proposed bridge alignment provides a semi-direct route for pedestrians and bicycles, likely capturing all users traveling from the north (National Zoo Loop/Beach Drive NW tunnel) and most, if not all, users from the south. Additional landscape features are proposed south of the bridge to dissuade trail users from continuing to use the narrow sidewalk across the Beach Drive NW bridge as a shortcut.

The recommended bridge type is a single span (approximately 105' length), concrete double-T section, supported by two abutments. There are two recommended options for the railings (presented herein). There are also two recommended options for the south approach (also presented herein). The preferred option is a fill embankment, which would be approximately 6—7 ft high near the abutment, descending to existing grade at a 5% longitudinal slope within approximately 75-ft. The volume of fill needed for the embankment is roughly 380 cubic yards.

A dark-colored palette is recommended in keeping with the context of Rock Creek Park's rustic quality. Proposed stonework would match existing facades of the tunnel portal and roadway bridge abutments, i.e. granite, schist, and gneiss, set in a random ashlar pattern with a rough face.





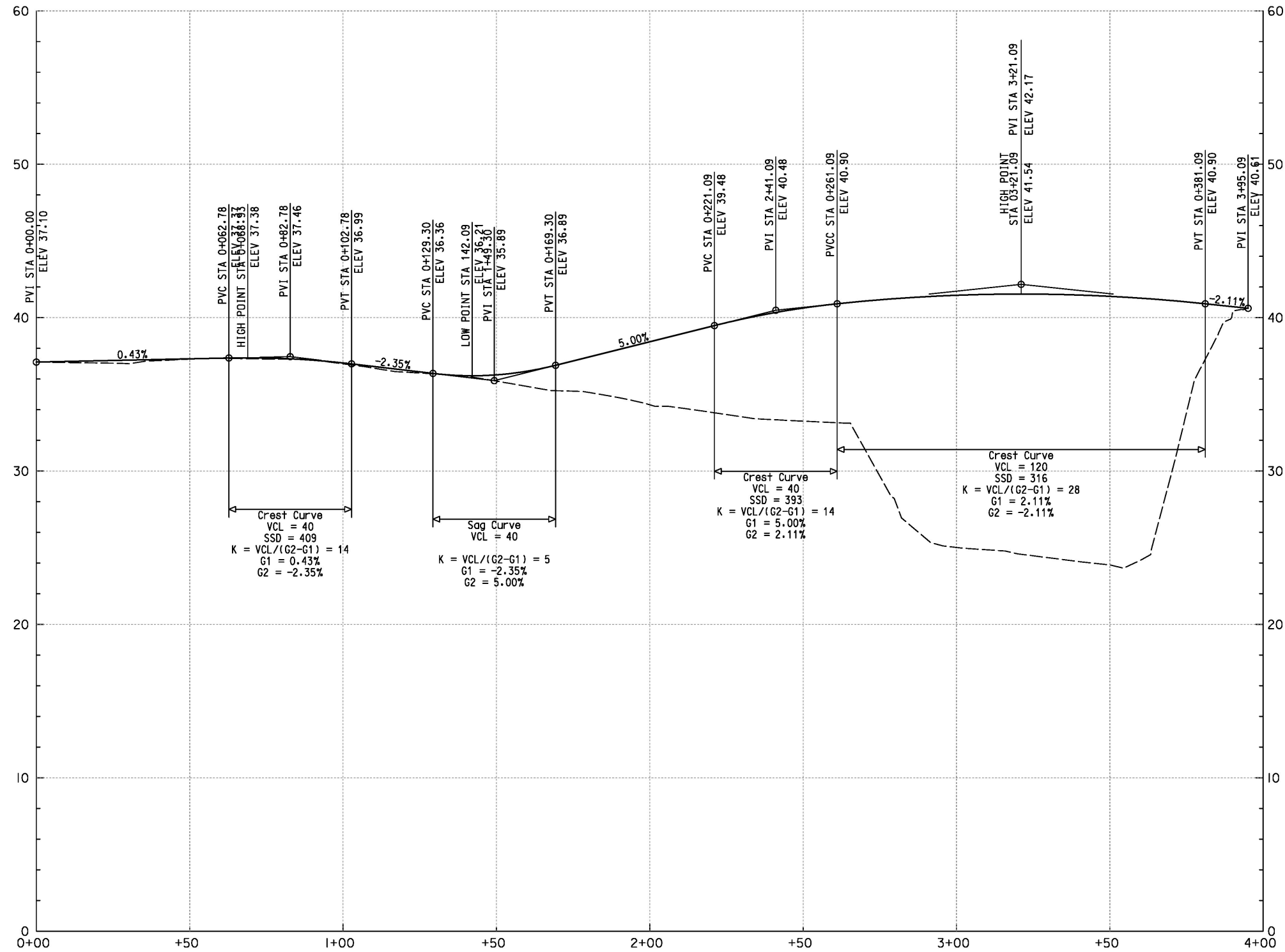
PROPOSED PEDESTRIAN BRIDGE
 OPTION 1 - STRAIGHT VERTICAL ALIGNMENT (PREFERRED)
 PROFILE VIEW

LINE	SURFACE	OFFSET
---	ROCK CREEK TRAIL	0.0000
Scaled 5.0000 Times Ver. Scaled 1.0000 Times Hor.		

DATE: 10-2016

SCALE: 1" = 20'

PROFILE
 PEDESTRIAN BRIDGE
 MODIFIED EA/FONSI ALTERNATIVE



PROPOSED PEDESTRIAN BRIDGE
 OPTION 2 - CAMBERED VERTICAL ALIGNMENT
 PROFILE VIEW

LINE	SURFACE	OFFSET
---	ROCK CREEK TRAIL	0.0000

Scaled 5.0000 Times Ver.
 Scaled 1.0000 Times Hor.

DATE: 10-2016	SCALE: 1" = 20'
PROFILE PEDESTRIAN BRIDGE MODIFIED EA/FONSI ALTERNATIVE	

Proposed Pedestrian Bridge Elevation View

Daytime, Post & Cable



Proposed Pedestrian Bridge Elevation View

Nighttime, Post & Cable with lighting



Proposed Pedestrian Bridge Deck View

Daytime, Post & Cable



Proposed Pedestrian Bridge Deck View

Nighttime, Post & Cable with lighting



Proposed Pedestrian Bridge—Abutment and Superstructure Detail View

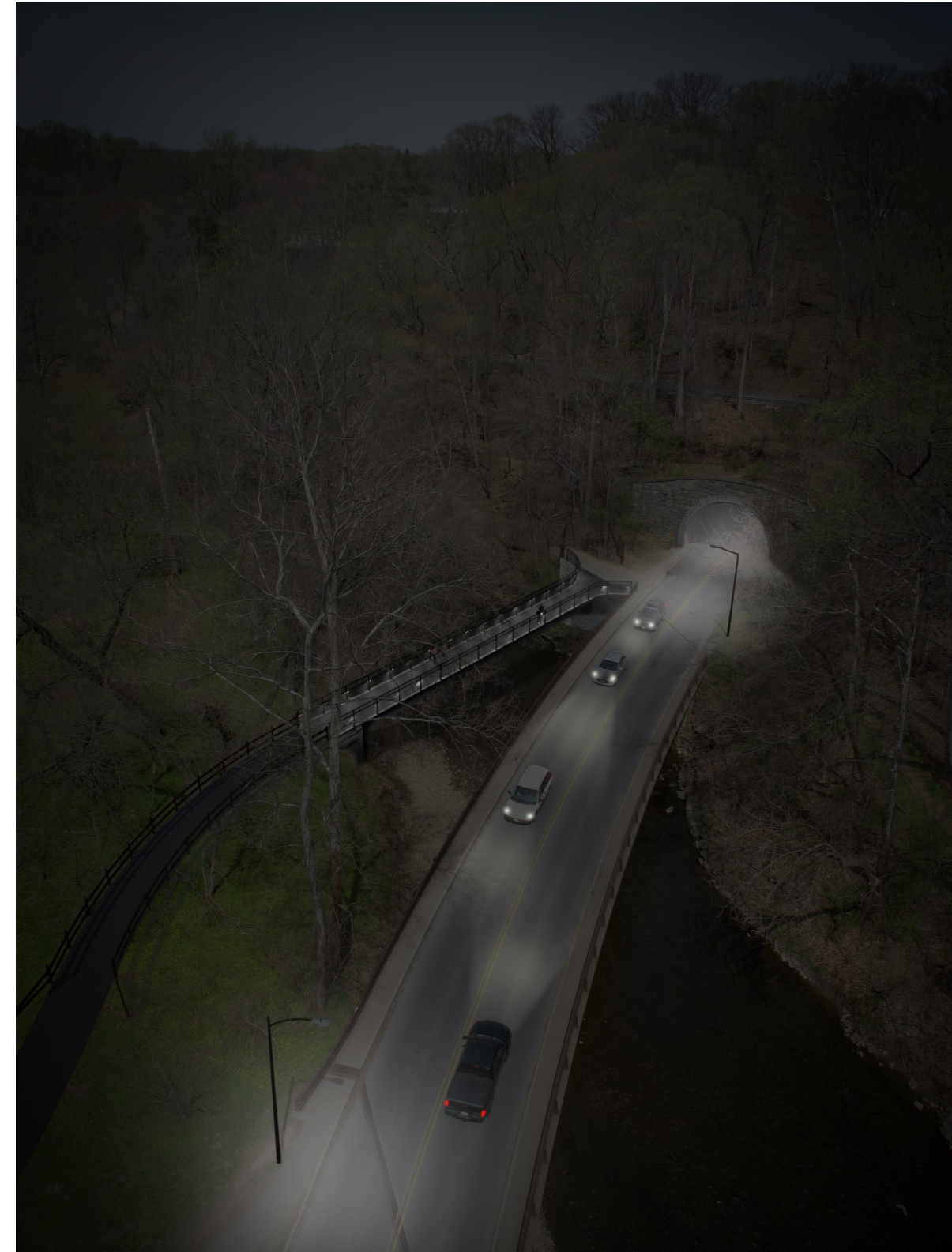


Proposed Pedestrian Bridge Aerial View from Duke Ellington Bridge

Daytime, Post & Cable



Nighttime, Post & Cable with lighting

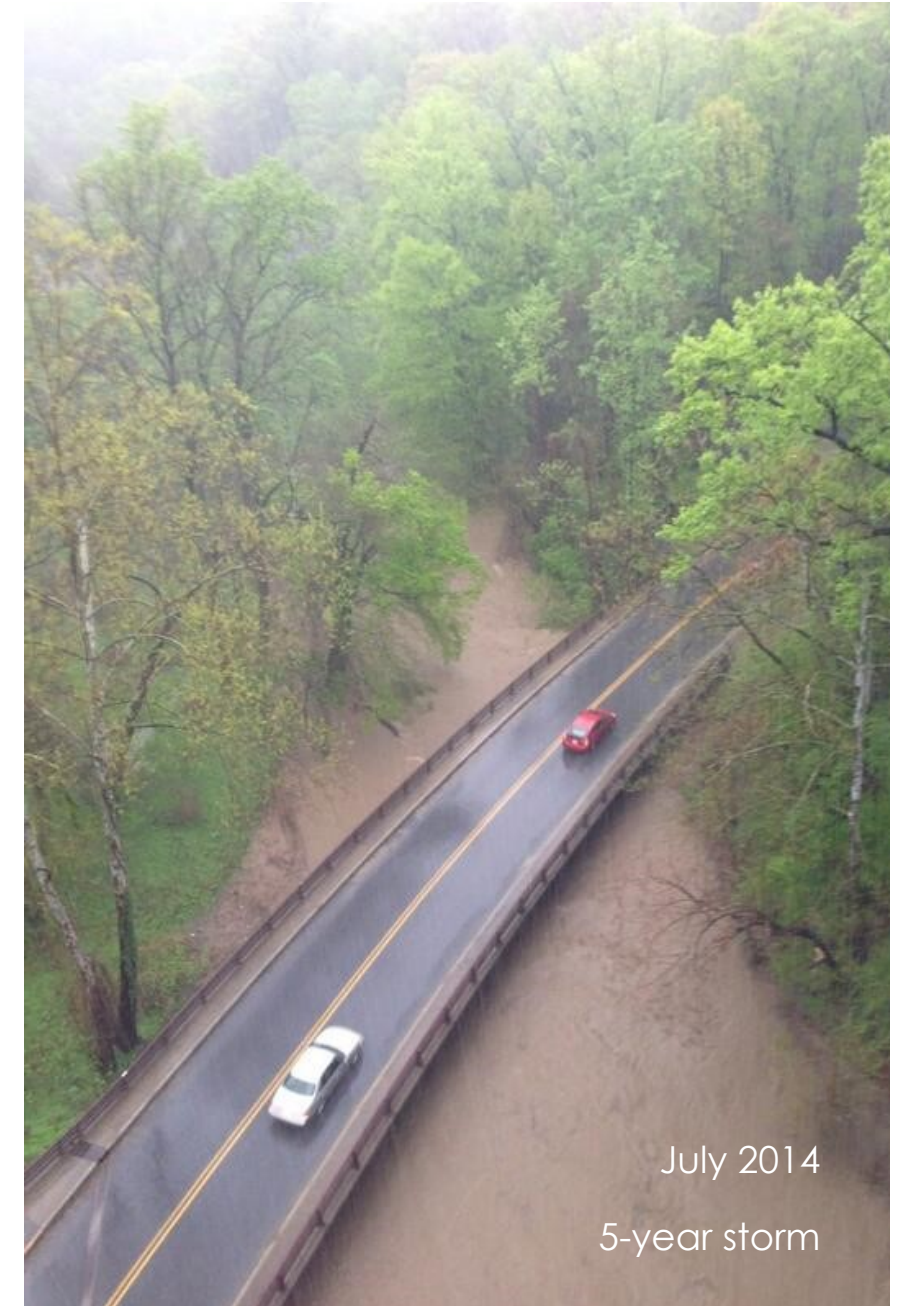


Floodplain and Bridge Hydraulics

The proposed pedestrian bridge over Rock Creek will be located slightly upstream from the existing Beach Drive bridge. The bridge will be located within Zone AE, a Special Flood Hazard Area (SFHA) of Rock Creek. There is no FEMA administrative floodway on this portion of Rock Creek. The drainage area is approximately 74.6 square miles. The current FEMA 100-year flood elevation at the existing Beach Drive bridge is approximately elevation 47.5, which would overtop Beach Drive by approximately 7 feet. Flood elevations higher than about 38-ft will overtop the roadway west of the existing roadway bridge.

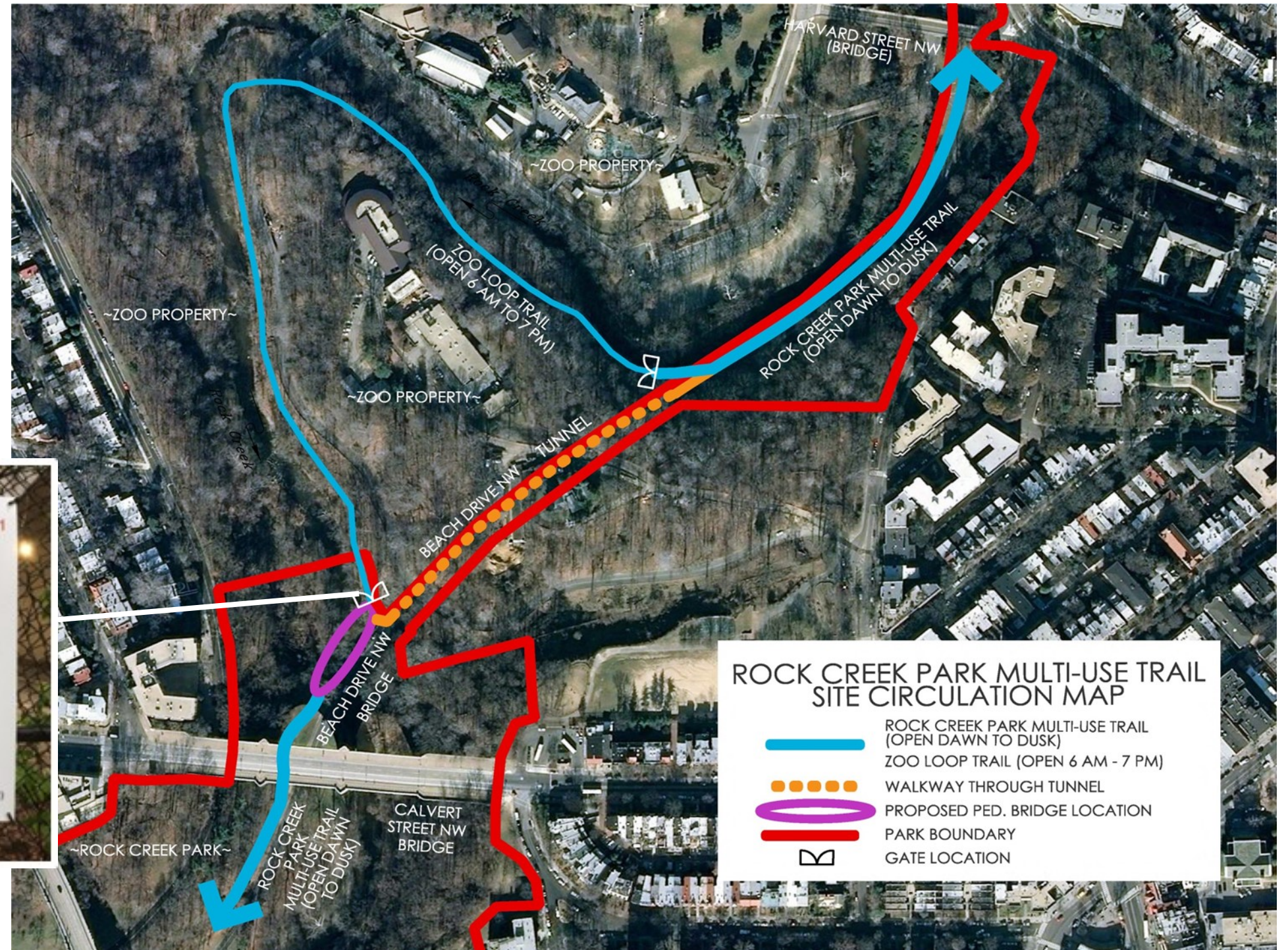
The previous modeling of the pedestrian bridge located immediately adjacent to the existing Beach Drive bridge resulted in an increase in the 100-year flood elevation of 0.1'. The floodplain elevation increase ranging from 0.1' to 0.0' extended upstream from the bridge for approximately 2.0 miles. The currently proposed location of the pedestrian bridge, located further upstream will cause additional impacts which will likely increase the 100-year flood elevation by slightly more than 0.1'.

Due to variances to the floodplain and base flood elevations caused by the proposed bridge review and approval from DOEE and FEMA will need to be approved prior to issuance of a building permit by DCRA.



Trail Access

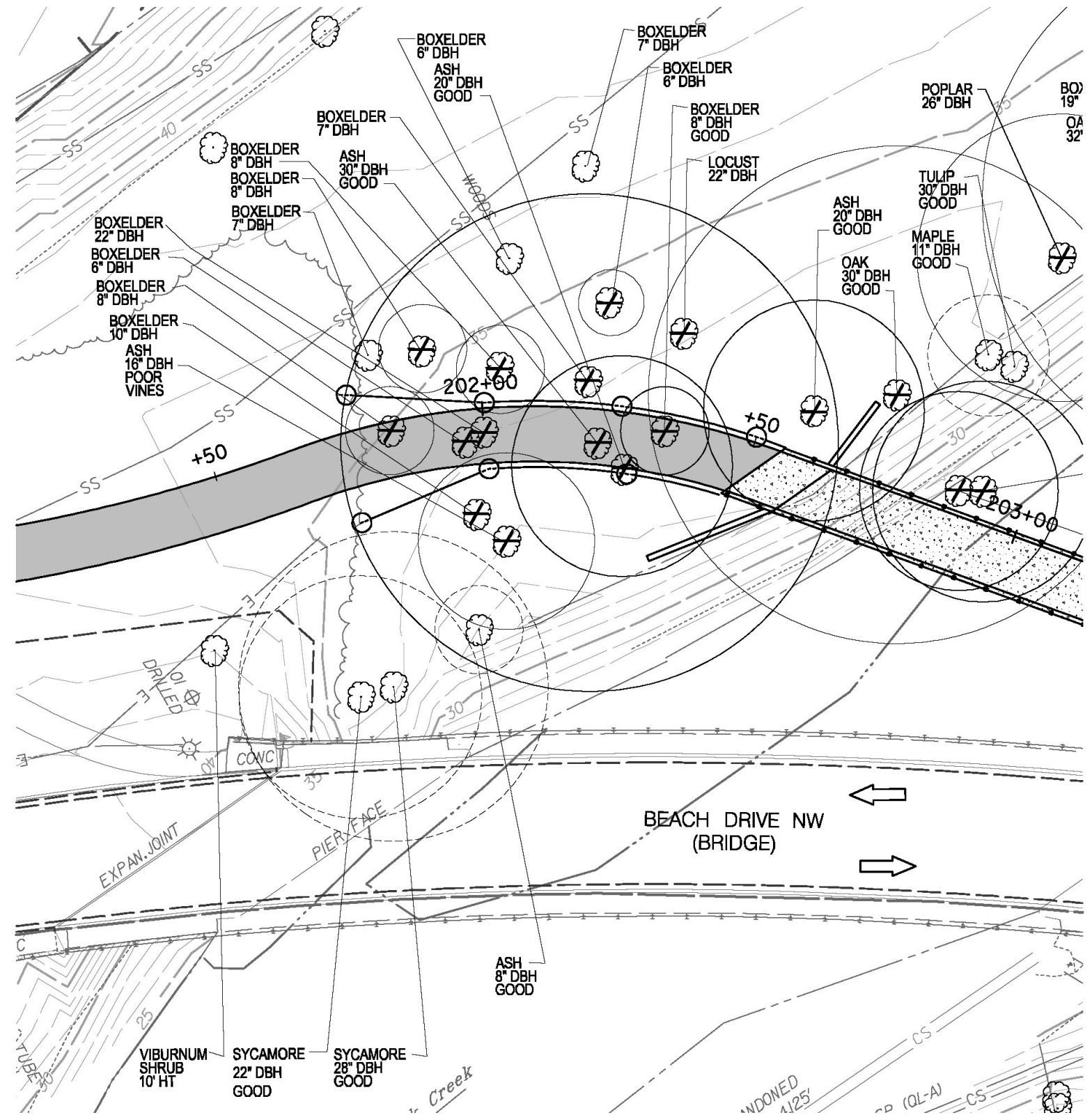
The National Zoo property is located to the north of the proposed pedestrian bridge site. There is an important trail link through the property. However, due to zoo regulations, the trail must be closed after 7 pm. Trail users must use the tunnel walkway for passage once zoo trail access is closed. Rock Creek Park and the trail are closed between dusk and dawn.



Tree Impacts

It is estimated that a total of fifteen trees, mostly boxelder and ash, would be impacted by a fill embankment footprint leading to the south pedestrian bridge abutment. The figure to the right depicts a plan view of surveyed tree locations, species, sizes, condition, and critical root zones. The 'x' symbols represent tree removals.

The proposed alignment preserves two mature sycamore trees located near roadway bridge abutment. These trees help to provide separation between the roadway and trail.



Endangered Species

According to the US Fish and Wildlife Service, the Hay's Spring amphipod (*Stygobromus hayi*), a federally endangered species, occurs in the vicinity of the proposed project within the boundaries of the Smithsonian's National Zoo. The US Fish and Wildlife Service recommended against alignments located further upstream due to the potential for construction related impacts i.e. tree clearing and an increase in unauthorized entry into the habitat area.



The Hay's spring amphipod is a small, aquatic crustacean that resembles a very tiny shrimp.



Platanus occidentalis
American Sycamore



Magnolis virginiana
Sweetbay Magnolia



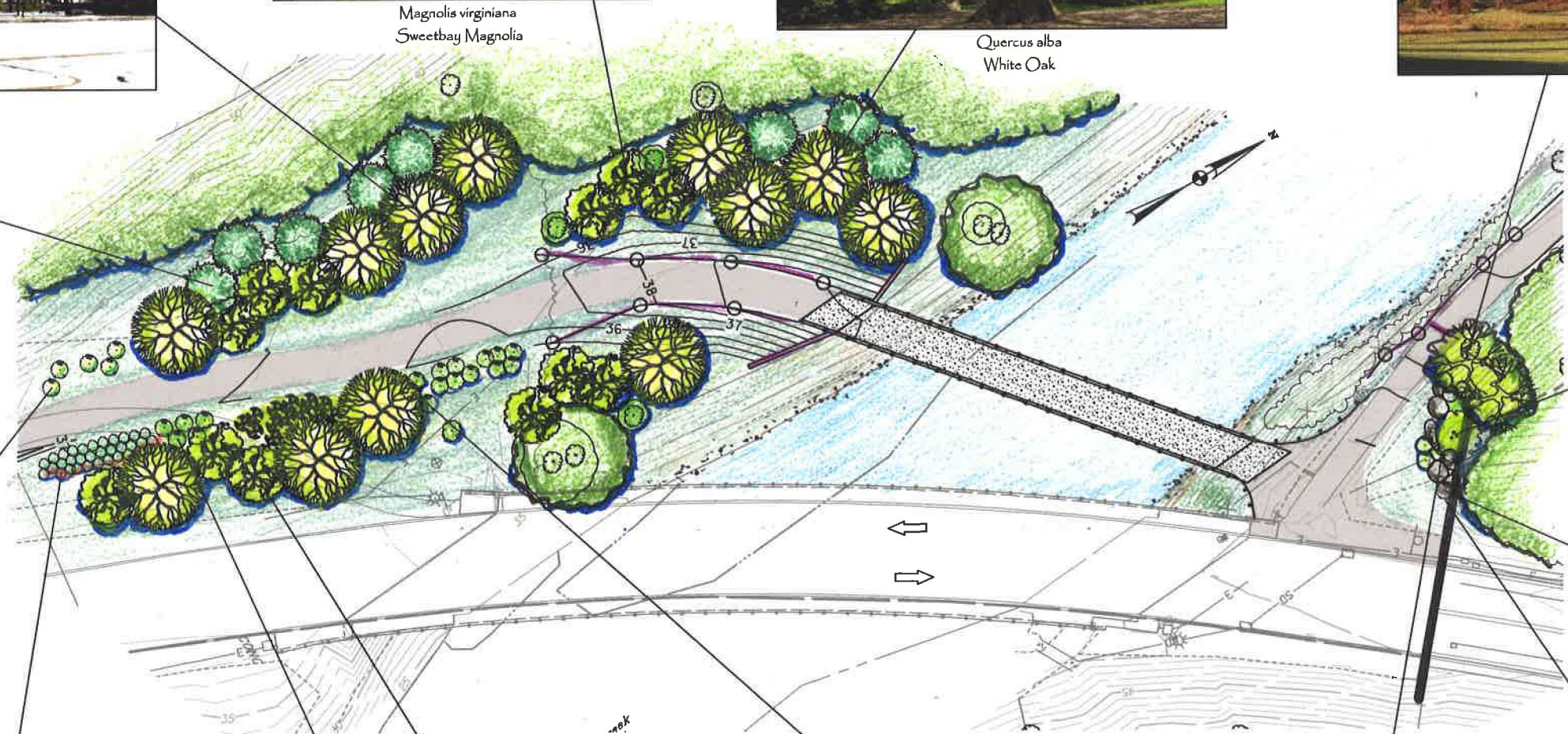
Quercus alba
White Oak



Quercus falcate
Southern Red Oak



American Holly
Ilex opaca



American Hornbeam
Carpinus caroliniana



Hamamelis xintermedia
Witchhazel



Flowering Dogwood
Comus florida



Rhus aromatica 'Low-gro'
Fragrant Sumac



Viburnum Triloba 'Compactum'
American Cranberrybush Viburnum



Cercis Canadensis
Eastern Redbud



Clethra anifolia
Summersweet Clethra



Christmas Fern
Polystichum acrostichoides



Creeping St. Johnswort
Hypericum calycinum

DATE: 10-2016 SCALE: 1" = 20'

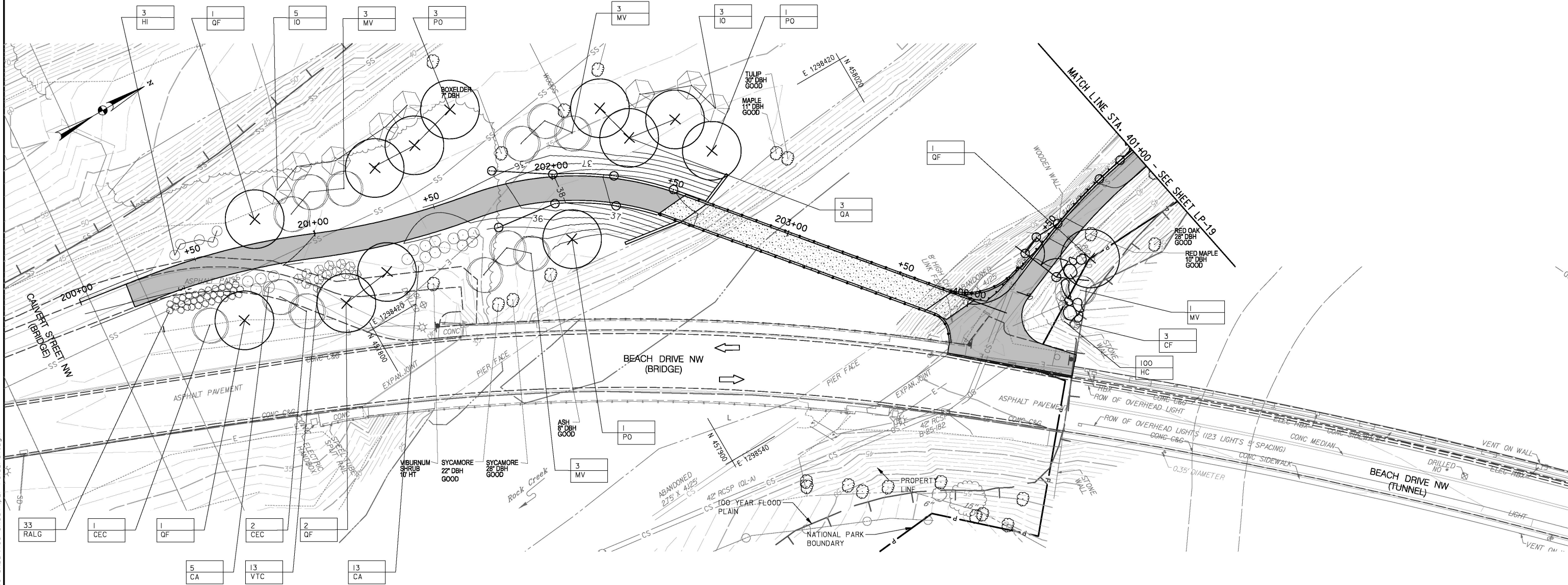
PLANT PHOTO EXHIBIT

20' 0 20' 40'
HORIZONTAL SCALE: 1" = 20'

REG	STATE	PROJECT	SHEET NO.	TOTAL SHEETS
3	D.C.		SLP-18S	248

LANDSCAPE TREE SCHEDULE - THIS SHEET ONLY

SYM	QTY	BOTANIC NAME	COMMON NAME	SPACING	SIZE	ROOT
TREE						
CEC	3	CERCIS CANADENSIS	EASTERN REDBUD	15' O.C.	2' CAL.	B&B
CF	3	CORNUS FLORIDA	FLOWERING DOGWOOD	15' O.C.	2' CAL.	B&B
IO	8	ILEX OPACA	AMERICAN HOLLY	30' O.C.	6' HT.	B&B
MV	10	MAGNOLIA VIRGINIANA	SWEETBAY MAGNOLIA	15' O.C.	2' CAL.	B&B
PO	5	PLATANUS OCCIDENTALIS	SYCAMORE	30' O.C.	3' CAL.	B&B
QA	3	QUERCUS ALBA	WHITE OAK	30' O.C.	3' CAL.	B&B
QF	5	QUERCUS FALCATE	SOUTHERN RED OAK	30' O.C.	3' CAL.	B&B
SHRUBS						
CA	18	CLETHRA ALNIFOLIA	SUMMERSWEET	4' O.C.	3 GAL.	CONT.
CS	6	CORNUS SERICA	REDOsier DOGWOOD	3' O.C.	5 GAL.	CONT.
HI	5	HAMAMELIS XINTERMEDIA	WITCHHAZEL	15' O.C.	5 GAL.	CONT.
RALG	33	RHUS AROMATICA 'LOW-GRO'	LOW-GRO FRAGRANT SUMAC	3' O.C.	3 GAL.	CONT.
VTC	13	VIBURNUM TRILOBA 'COMPACTUM'	COMPACT AMERICAN CRANBERRYBUSH VIBURNUM	4' O.C.	5 GAL.	CONT.
GROUND COVER						
HC	100	HYPERICUM CALYCYNUM	CREeping ST. JOHNswORT	8' O.C.	2' PLUG	POT



LANDSCAPE LEGEND

- PR. PEDESTRIAN STRUCTURE
- PR. ASPHALT PAVEMENT
- P1 POROUS ASPHALT
- PROPOSED MAJOR TREE
- PROPOSED MINOR TREE
- REFORESTATION AREA

NOT FOR CONSTRUCTION



810 Gleneagles Court, Suite 300
 Baltimore, MD 21286
 www.stantec.com

NO.	DESCRIPTION	NAME	DATE
REVISIONS			

D.C. DEPARTMENT OF TRANSPORTATION
 INFRASTRUCTURE PROJECT MANAGEMENT ADMINISTRATION
 PROJECT MANAGEMENT DIVISION

ROCK CREEK PARK MULTI-USE
 TRAIL REHABILITATION AND
 PEDESTRIAN BRIDGE

DATE: 08-26-2016	SCALE: 1" = 20'	LP-18
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Proposed Portal Wall Extension Plan

The proposed portal wall extension will improve sight distance between trail users by reducing the existing grade, but will create a broad, flat space. To preserve the woodland character of the site, boulders and plantings, include Christmas Fern, Creeping St. Johnswort, Flowering Dogwood, American Hornbeam, and Southern Red Oak, are proposed to frame the appearance of the portal wall.



Proposed Portal Wall Extension—View from Beach Drive NW



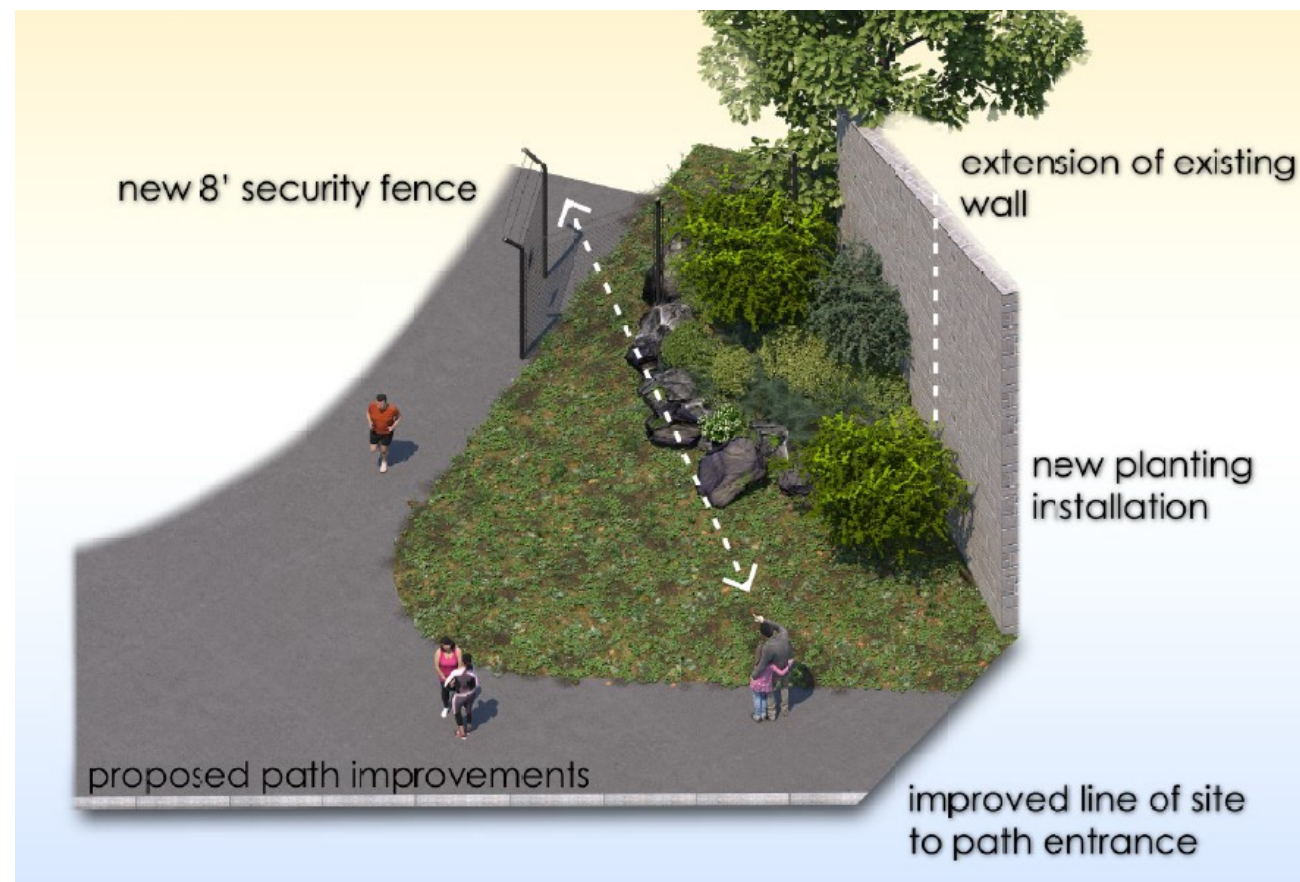
Existing



Proposed

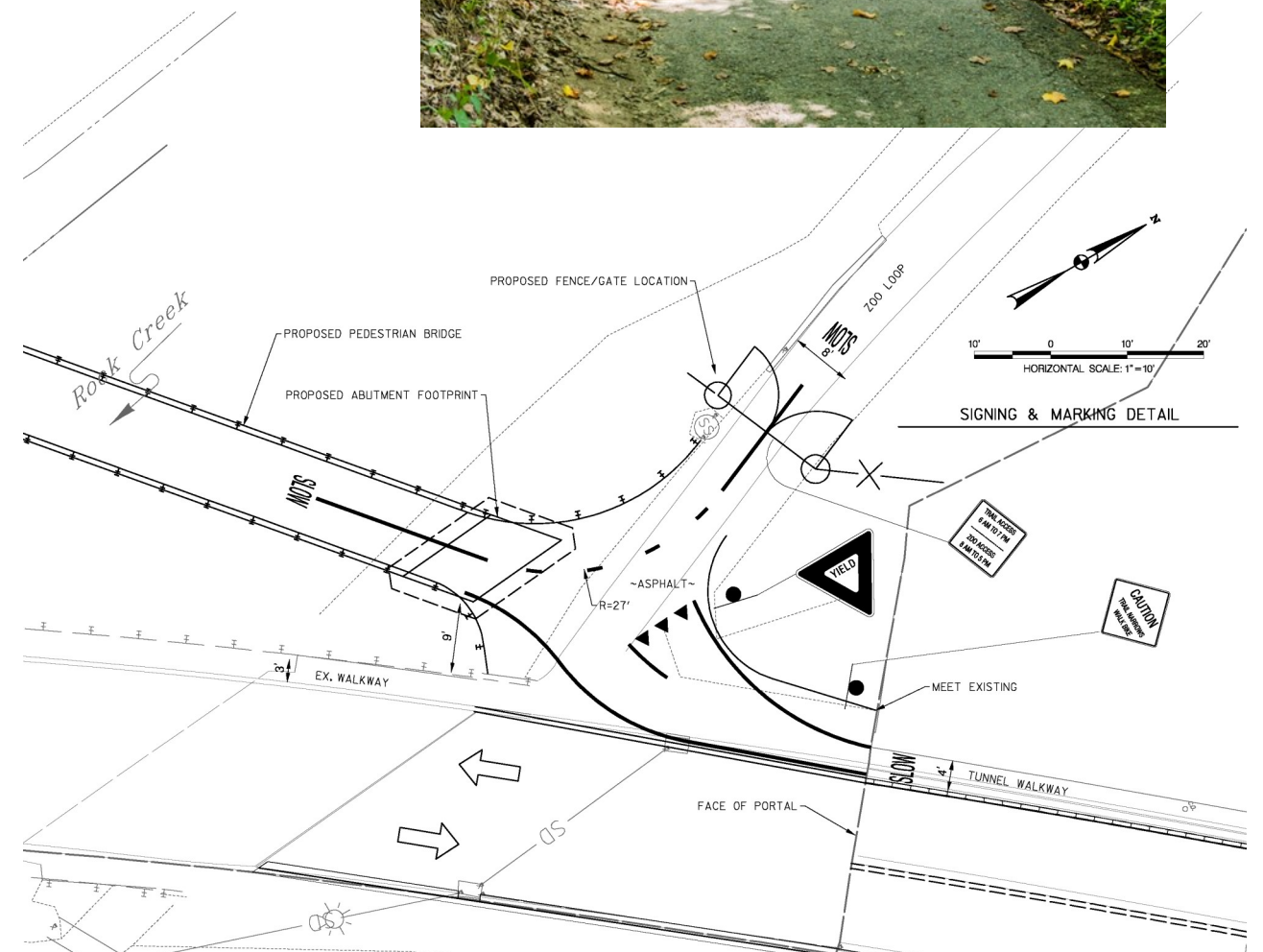
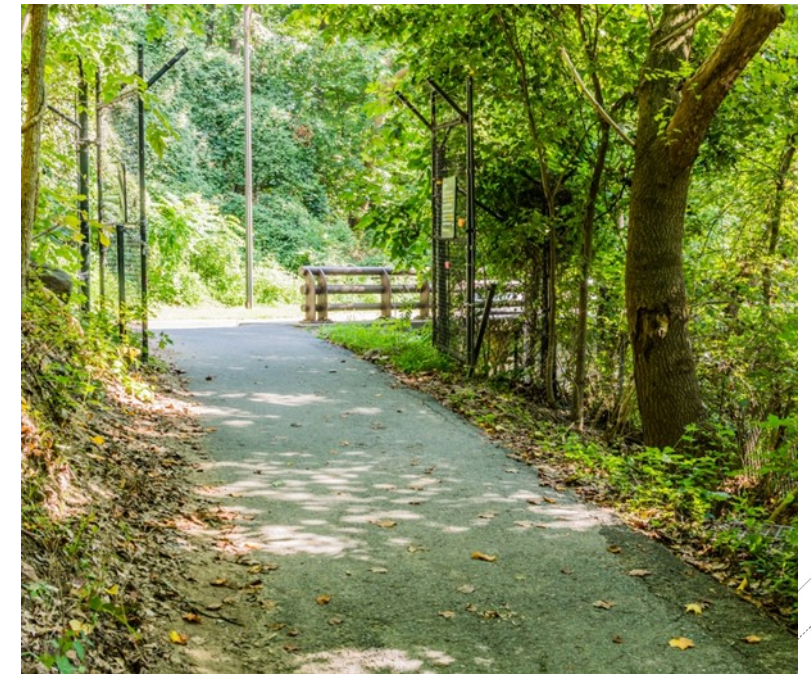
Pedestrian and Bicycle Safety

The existing condition at the trail junction between the zoo loop, tunnel walkway, and bridge sidewalk is hazardous due to high trail user volumes, poor visibility, and tight space constraints. When the proposed pedestrian bridge is completed, trail users will continue to converge in a three-way intersection. Special consideration has been given to providing safety measures and sight line improvements at this junction.



This conceptual perspective sketch depicts how a portal wall extension could be integrated with the tunnel portal to improve sight lines between trail users.

Poor visibility looking along Zoo Loop Trail toward Beach Drive NW, especially with foliage.



Based on the proposed improvements, it is recommended that the trail junction be signed and marked as an intersection where trail users traveling along the bridge and zoo loop have the right-of-way.

Existing 42" Combined Sewer Protection

An existing 42" combined sewer runs parallel to Rock Creek along the northeast bank. According to DC Water design manual, structural footings and piles must maintain a minimum 5-ft clearance from existing sewers.

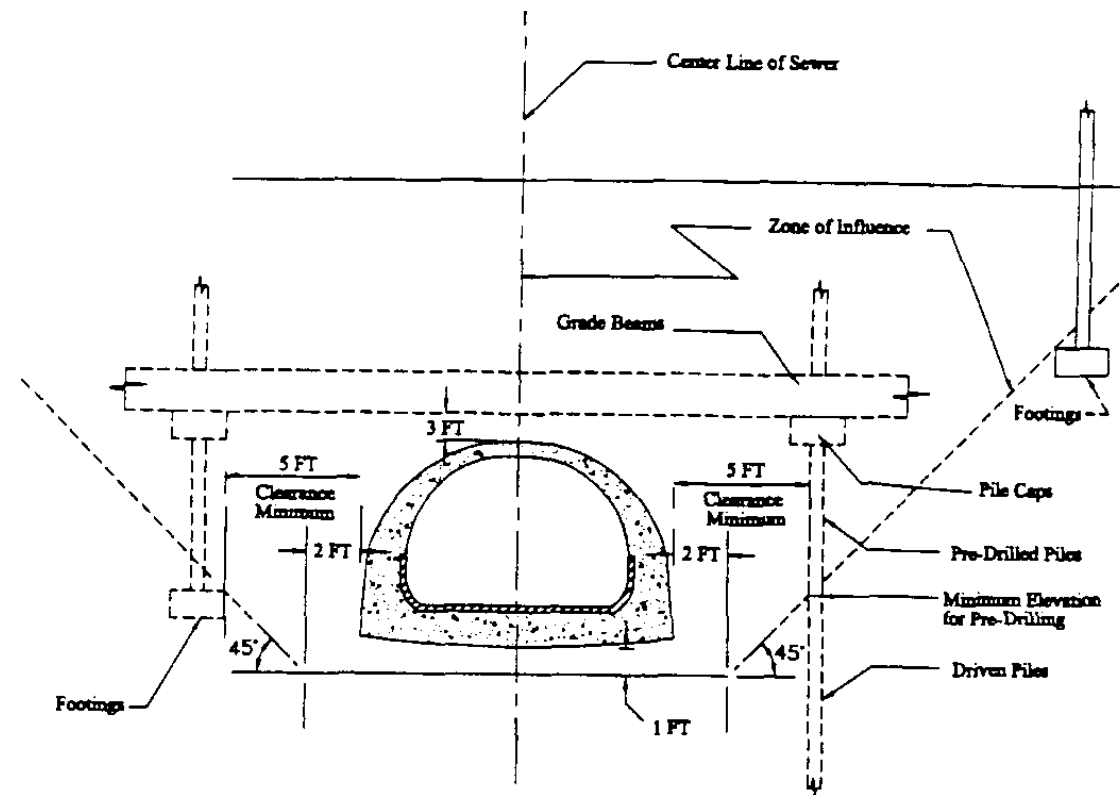
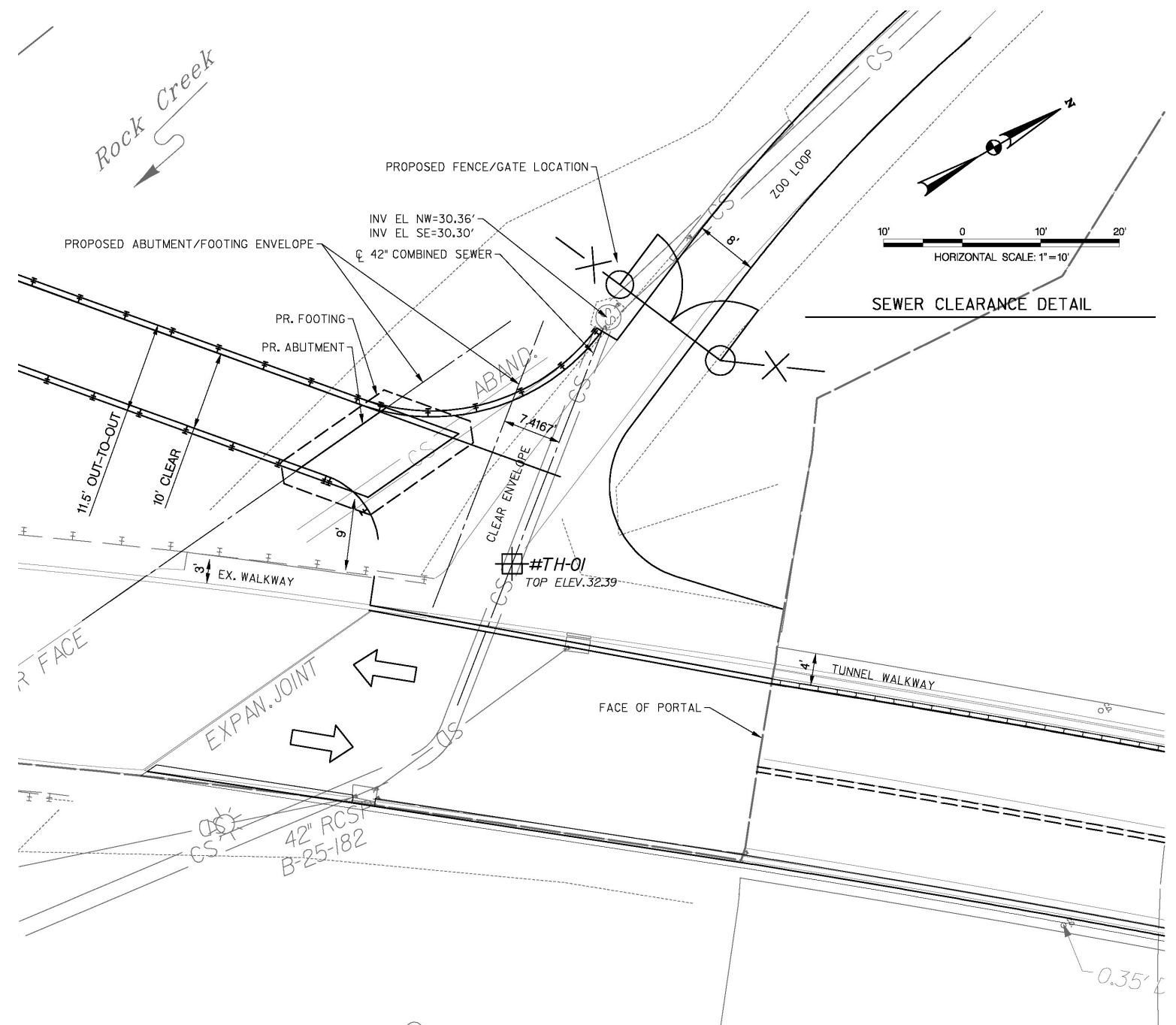


Figure from DC Water design manual depicting clearance requirements (above); Plan view detail depicting proximity of the proposed bridge to the existing 42" combined sewer (right).



Fence Details

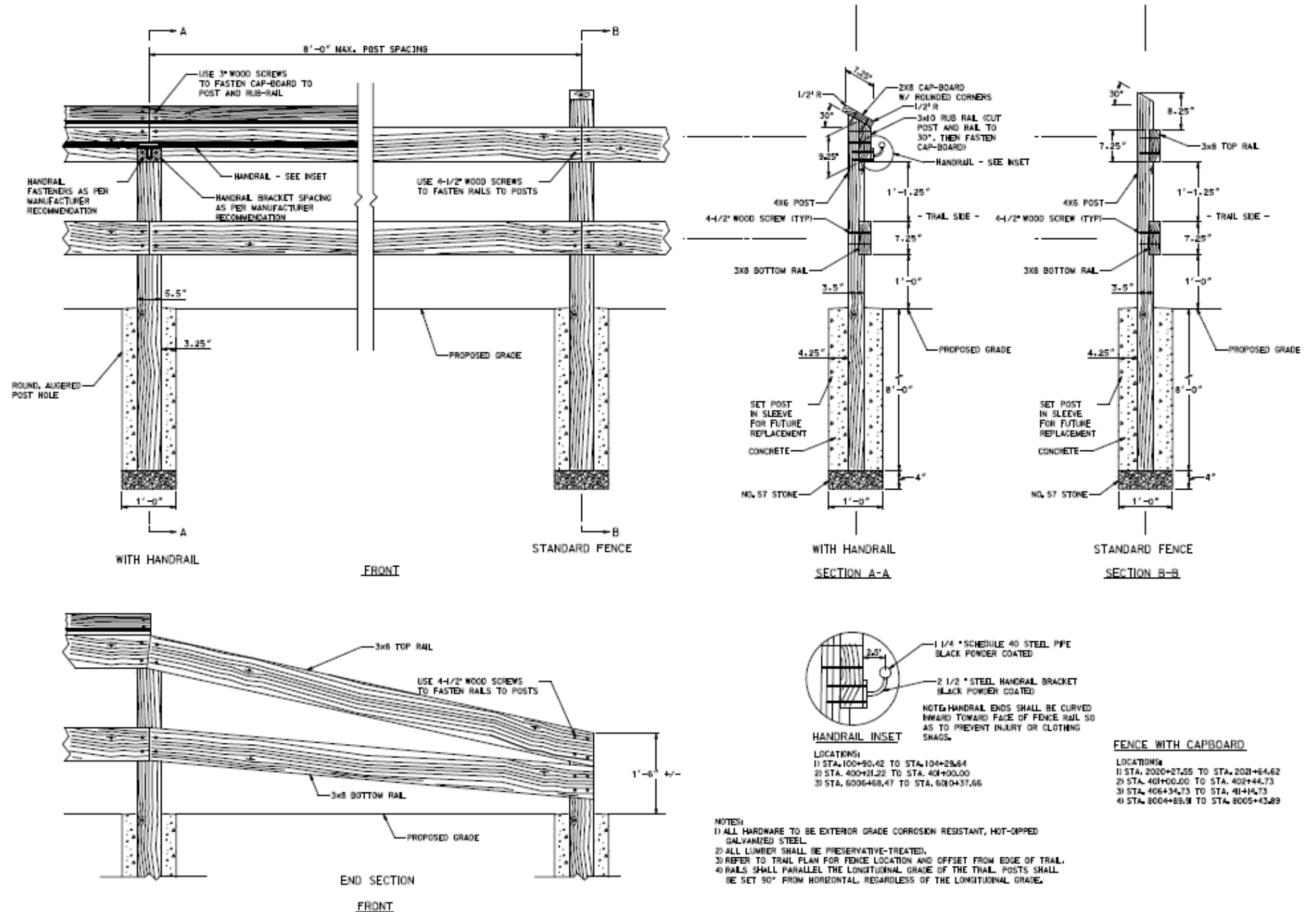
To improve safety, a 42 inch post-and-rail timber fence is proposed along trail segments with steep, adjacent slopes. The timber fence will be designed for compatibility with bicycle and pedestrian use. Rails will be located on the trail side. The top rail will be aligned with the bicycle handlebar height. Based on discussions with NPS, lumber for the fence will be treated but will not be painted.



Typical Wooden Post & Rail fence located in Rock Creek Park (left); proposed fence design with variations (right)

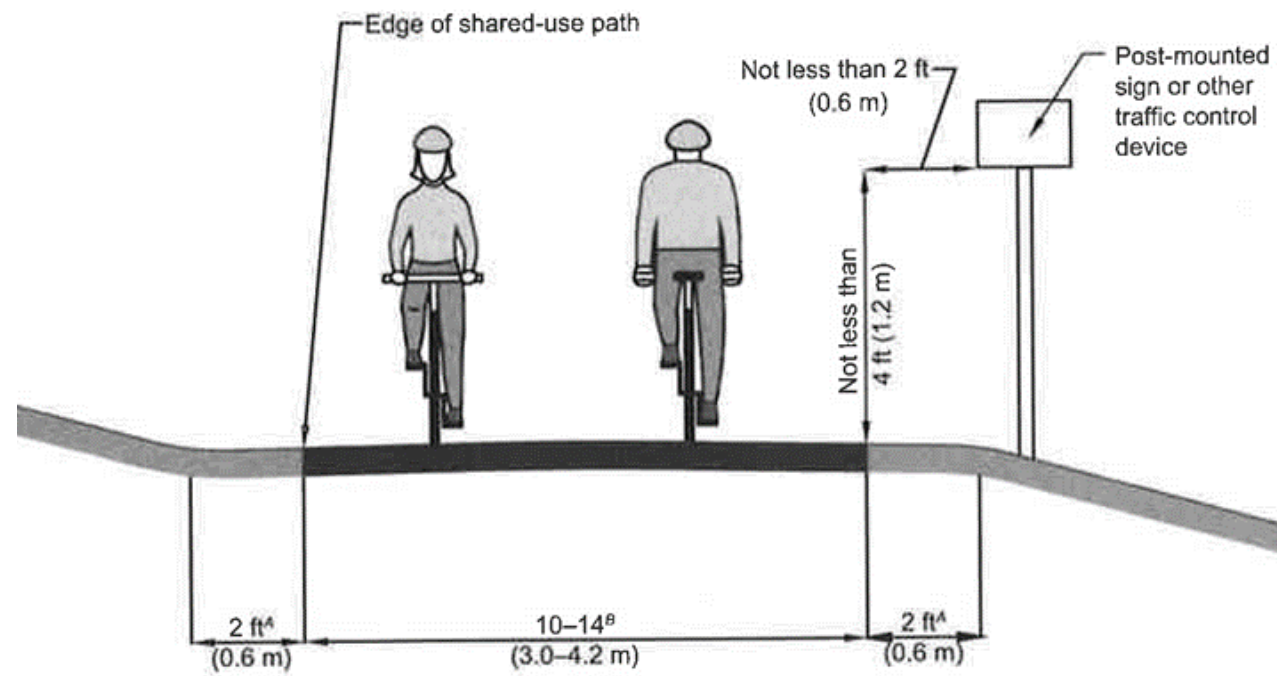


"Zoo Loop" Fence and Gate, locations to be adjusted and/or replaced

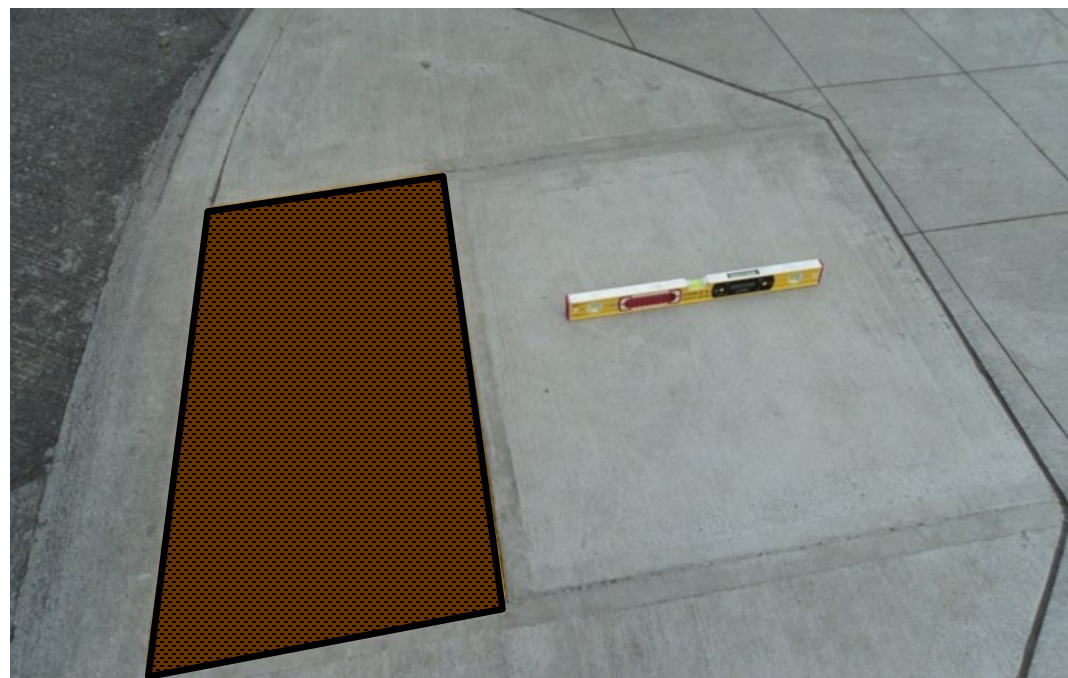


DETAIL A
42" POST-AND-RAIL WOODEN FENCE

Trail Elements



Typical Proposed Trail Section



Typical Curb/ADA Ramp



Existing Timber Edge Support



Typical NPS Wayfinding Sign

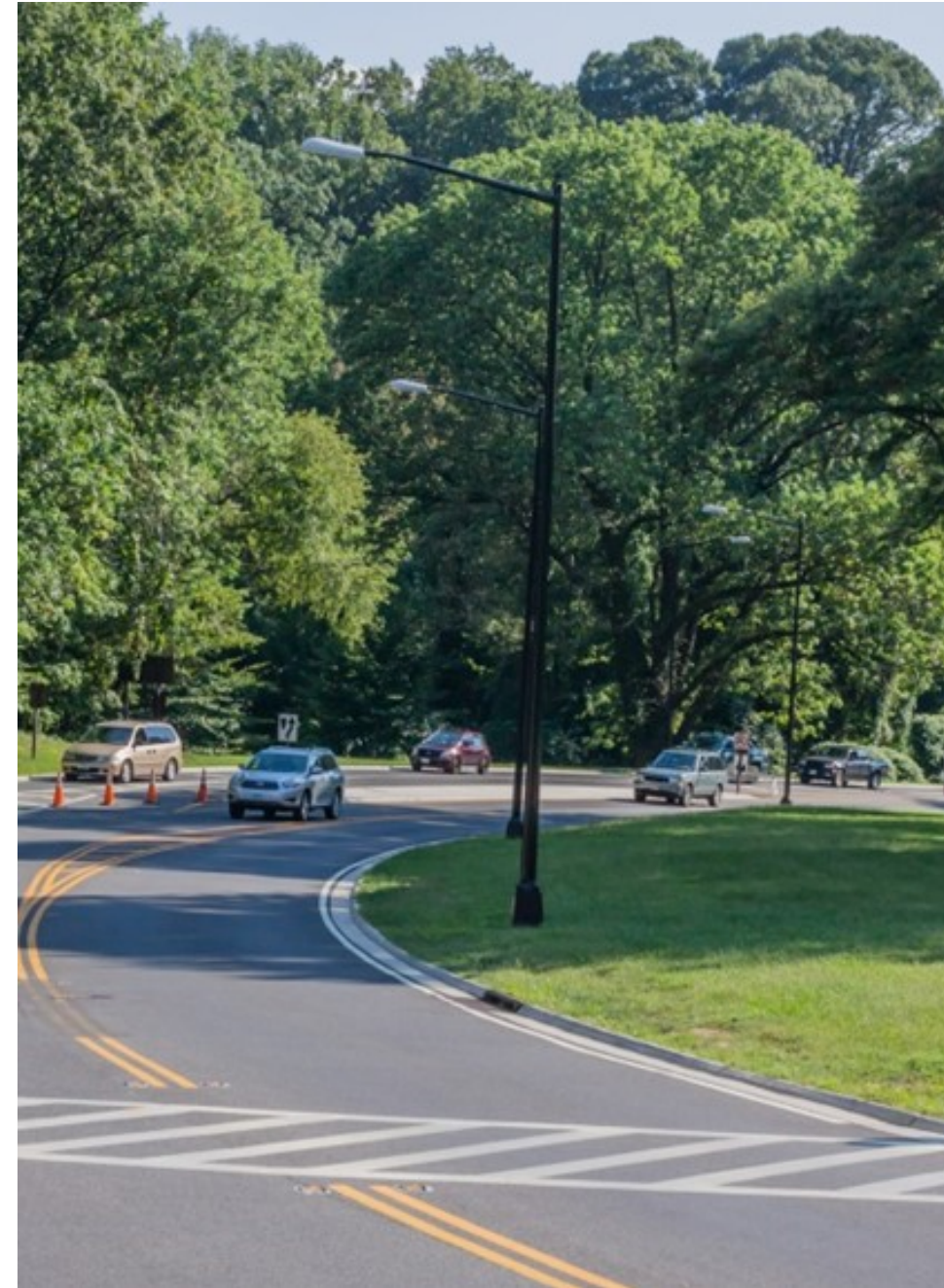
Roadway Elements



Existing Timber Steel Backed Timber Barrier



Existing Concrete Curb and Gutter



Existing Cobrahead Lighting

Proposed Stream Bank Stabilization Details

Vegetated geogrids are proposed to stabilize the eroding stream bank along Rock Creek adjacent to the proposed pedestrian bridge. The vegetated geogrid proposed is a combination of angular stone and soil fabric lifts which are designed to protect the trail/stream bank, prevent erosion, and to provide a natural look to integrate into the existing landscape.

The angular stone, or stone toe, mimics a natural process already occurring in Rock Creek where bedrock outcrops protect the toe of stream banks. The constructed stone toe is composed of a low wall of boulders with footers, partially buried into the streambed. Soil fabric lifts are installed on top of the angular stone to create the vegetated geogrid. Soil fabric lifts are a series of short soil terraces wrapped in biodegradable erosion control fabric. The use of terraces instead of a flat slope prevents slumping during the plant establishment period and allows the stream bank to stabilize more rapidly. The lifts are planted with fast-growing shrubs. For ease of installation and integrity of the soil lift matting, Live Stakes (dormant cuttings) or Tubelings (live stakes that have been partially grown) are used in lieu of container grown shrubs.



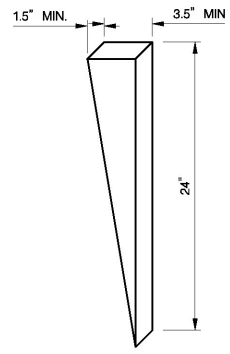
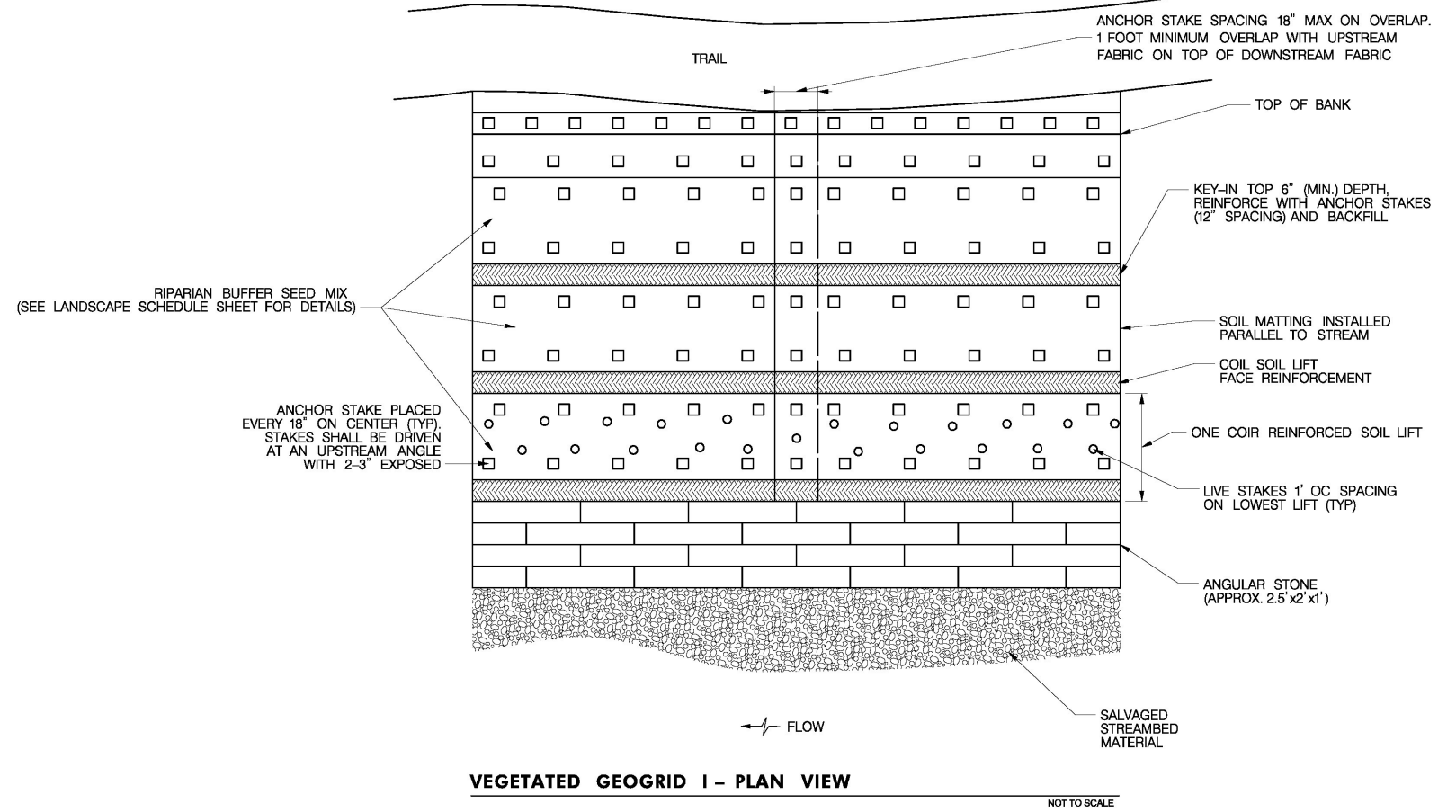
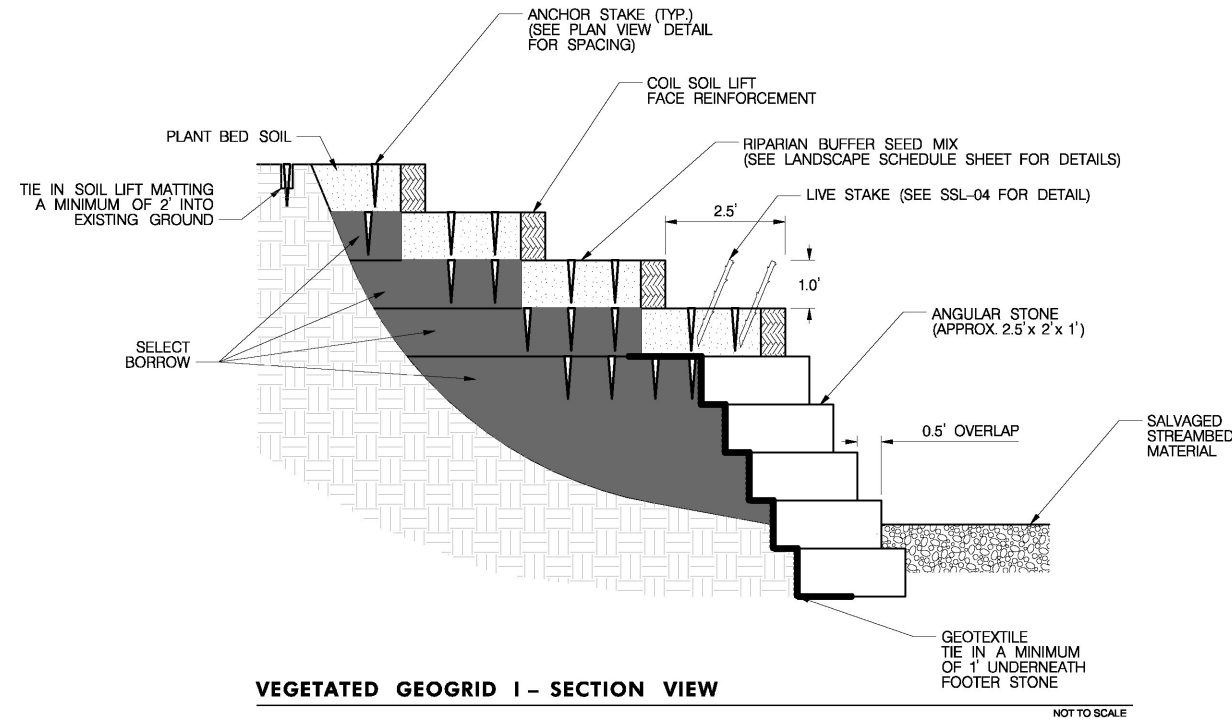
The northeast bank of Rock Creek in vicinity of the pedestrian bridge is eroding during heavy storms and undermining the existing trail.



Example plantings for similar improvement showing one of growth (left) and 3 years of growth (right). Proposed species include southern arrowwood, elderberry, silky dogwood, and black willow tubelings/live stakes.



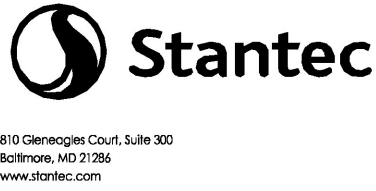
Example of materials installed at Georgetown Waterfront Park



NOTES:
1. ANCHOR STAKES SHALL BE TAPERED TWO FOOT LONG WOODEN STAKES CONSISTING OF STANDARD 2" X 4" WOODEN BOARDS CUT DIAGONALLY.

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NOT FOR CONSTRUCTION



NO.	DESCRIPTION	NAME	DATE
REVISIONS			

D.C. DEPARTMENT OF TRANSPORTATION
INFRASTRUCTURE PROJECT MANAGEMENT ADMINISTRATION
PROJECT MANAGEMENT DIVISION

ROCK CREEK PARK TRAIL
AND PEDESTRIAN BRIDGE
FINAL DESIGN SERVICES

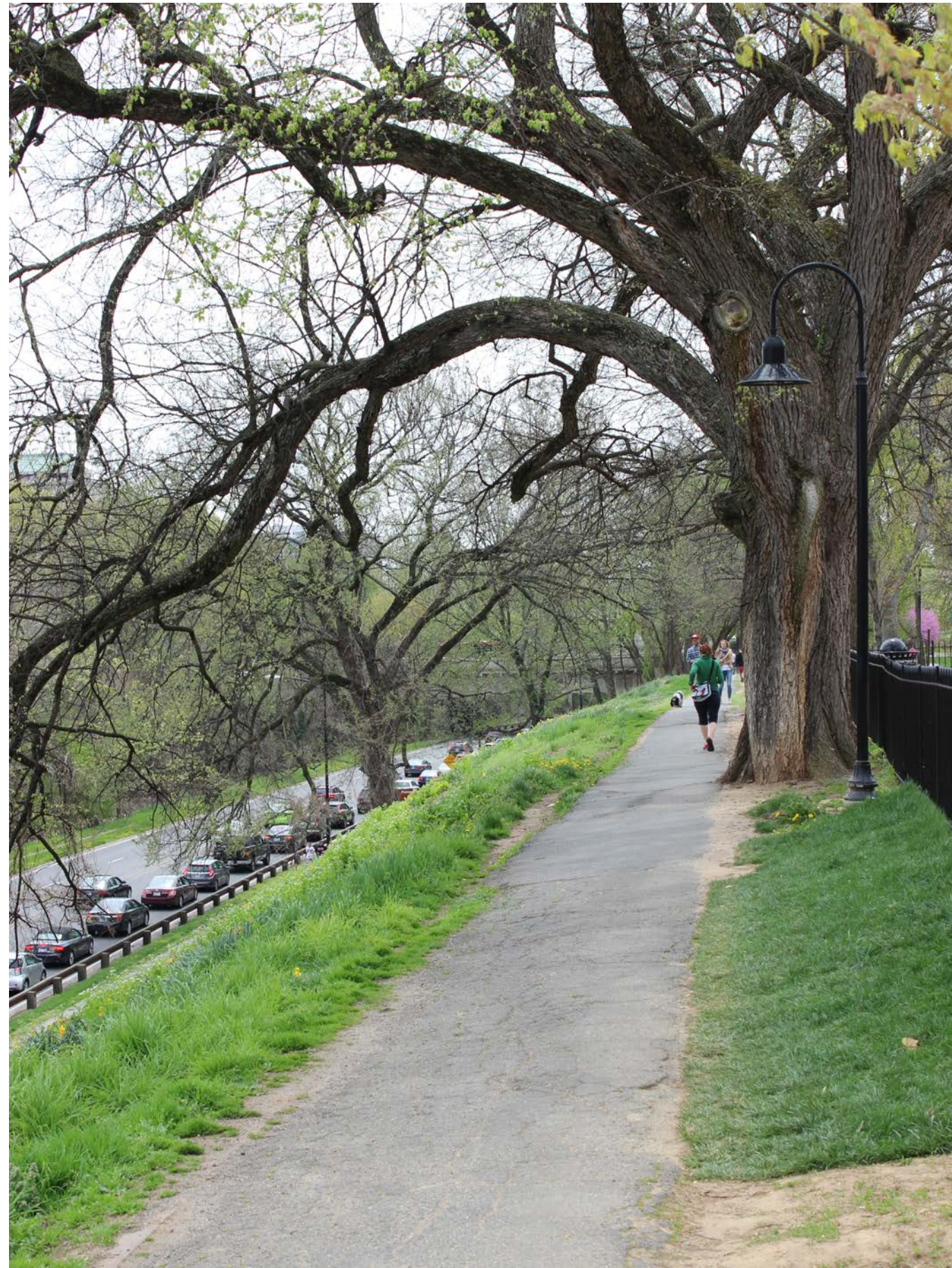
PROJECT ENG. _____
DESIGNED BY _____
CHECKED BY _____
DRAWN BY _____
PROJECT MGR. _____

08-26-16	SCALE: N.T.S.	SSG-04
STREAM BANK STABILIZATION DETAILS		DIVISION CHIEF
		DATE _____
		FILE _____
		SHEET 108 OF 223



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District Dept. of Transportation
Phone: 202-671-4584
Email: zahra.dorriz@dc.gov

Website:
<http://ddot.dc.gov/page/rock-creek-park-multi-use-trail-rehabilitation-project>



Rock Creek Park Multi-Use Trail Project

Rose Park Trail

November 2016



Presented to: **ANC 2E**



Presented by:



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Project Background

The Environmental Assessment / Finding of No Significant Impact (EA/FONSI) document for the Rock Creek Park Multi-Use Trail and Pedestrian Bridge Project was finalized in June 2014. The District of Columbia Department of Transportation (DDOT), in partnership with the National Park Service (NPS), is currently preparing final designs for the project. The EA/FONSI recommended the preferred Alternative 3: Trail Resurfacing and Widening, which included widening Rose Park Trail to 6 feet (Option B).



Rose Park

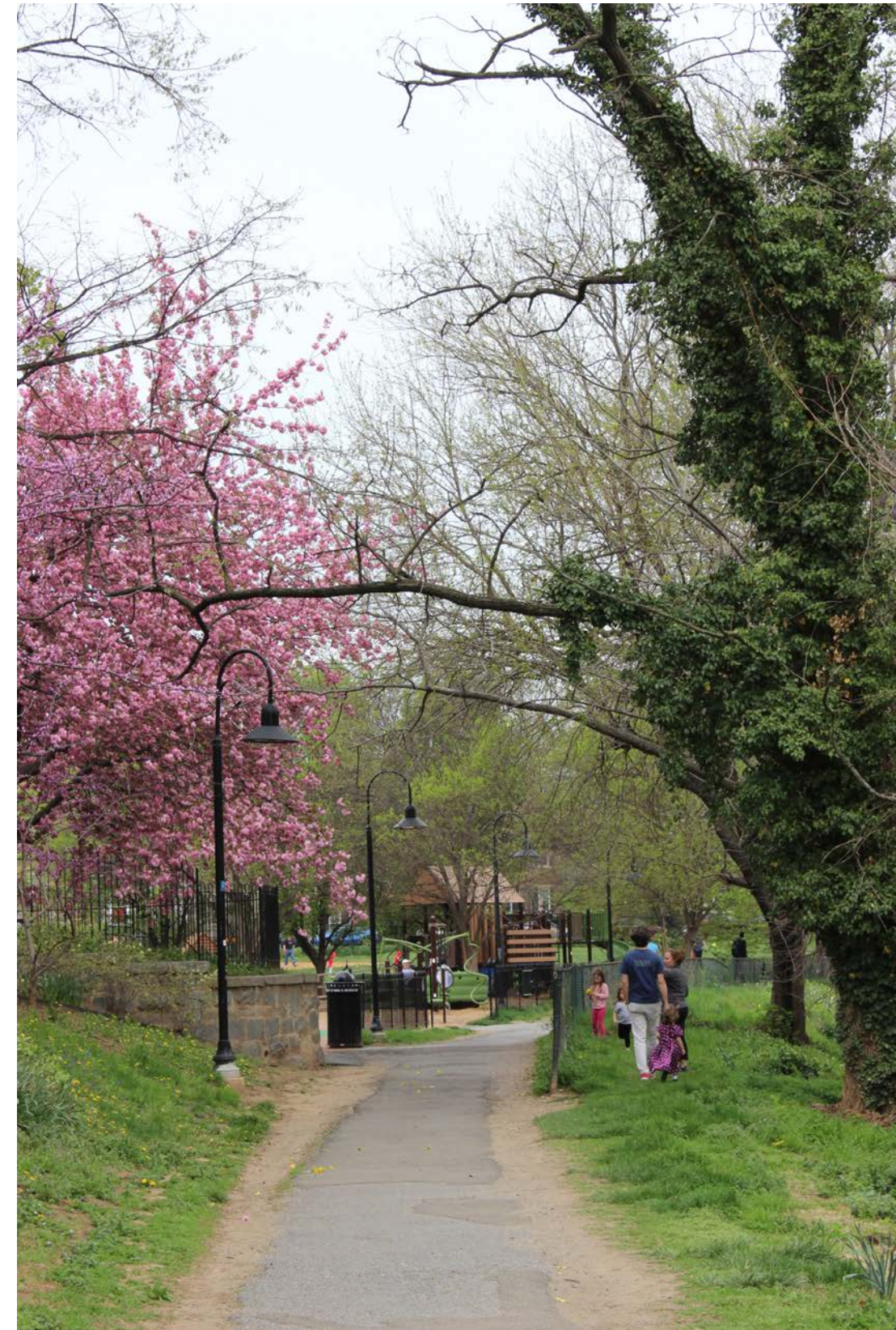
Existing Conditions

The existing Rose Park trail is located along the eastern boundary of the Old Georgetown neighborhood in Northwest Washington, DC. The trail is 1/3 mile (1,950 ft) with a southern terminus at M Street NW and a northern terminus at P Street NW. It is situated along a ridge, paralleling the west side of Rock Creek & Potomac Parkway. The existing trail ranges from 4-ft to 5-ft wide.

A large swath of property located to the immediate west of the trail was transferred from Rock Creek Park (US Reservation 360) to the District of Columbia for recreational purposes in 1972, however, the trail itself and the grassy areas to the north and south remain National Park land. The District of Columbia parcels are administered by the District Department of Parks and Recreation. There are two playgrounds, a recreation center, basketball courts, tennis courts, and a baseball field.

There is medium scale, decorative pedestrian lighting along the trail adjacent to the playgrounds. Black, decorative, metal picket fencing separates the trail from the playgrounds as well. A deteriorated chainlink fence separates the trail from the ridge slope.

Existing tree cover along the trail includes a grove at the south end composed of red oak, red maple, redbud, ginkgo, elm, sugar maple, and black locust trees. An important 44" elm tree is located at the approximate mid-point of the trail, providing shade to the adjacent playground. Additional trees located along the trail include walnut, cherry, white oak, pin oak, and red maple.



Rose Park Photos



Looking north along
Rose Park Trail to-
ward P Street NW



Looking south
along Rose Park
Trail with Rock
Creek & Potomac
Parkway to the east



Looking north along
Rose Park Trail near
Dumbarton Street
NW



Looking south
along Rose Park
Trail toward M
Street NW

Rose Park

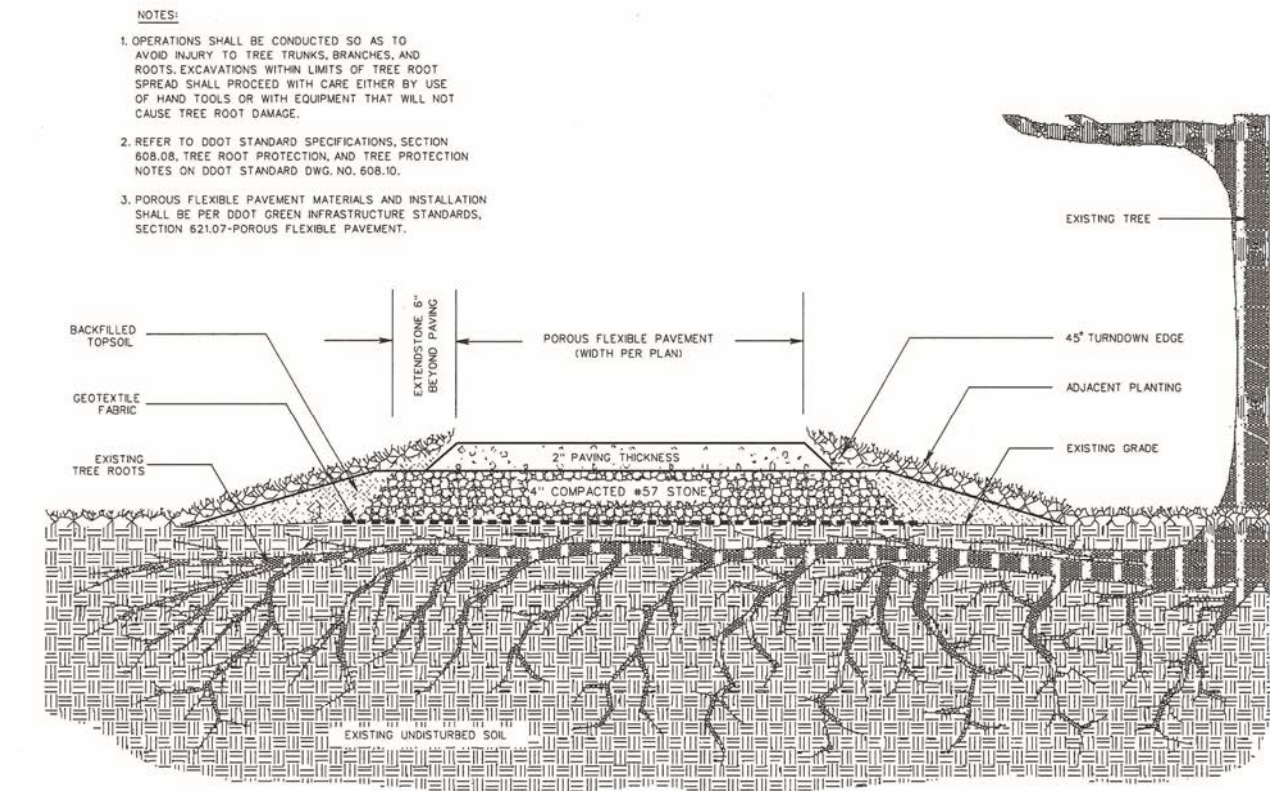
Proposed Improvements

Trail widening: The existing 4-ft to 5-ft wide trail will be reconstructed to a consistent 6-ft width. The proposed pavement section is porous asphalt. Flexi-pave is proposed for the segment of pavement within the critical root zone of the 44" Elm tree.



Example of porous asphalt pavement

Fencing: Safety fence is proposed for the length of trail immediately adjacent to the ridge. Two options are currently proposed. Option 1 (preferred) is a black vinyl-coated chainlink fence. Option 2 is a timber post & rail fence with wire mesh or chainlink fencing.



Flexi-pave is proposed within the critical root zone of the 44" Elm located near the mid-point of the trail.



Option 1: Black vinyl-coated chainlink fence, 3-ft to 4-ft height

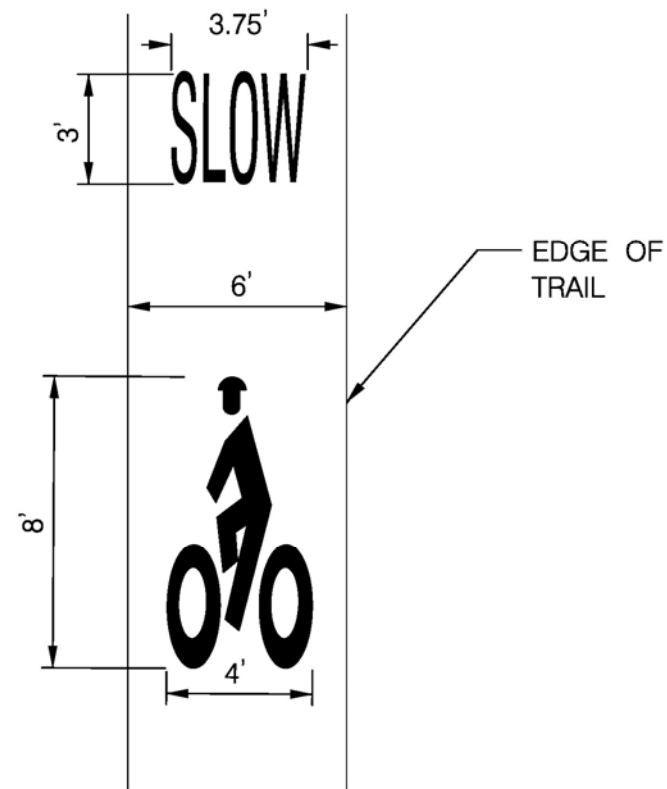


Option 2: Timber Post & Rail, chainlink combination

Rose Park

Proposed Improvements

Signing & Marking: Based on input from the community, improvements such as rumble strips and caution signs were considered for enhancement of safety between pedestrians and bicycles. The National Park Service proposes only pavement markings be used to discourage bicyclists from riding too fast adjacent to the playgrounds.



Two variations of pavement marking stencils that could be used on Rose Park Trail to caution bicyclists riding near the playgrounds.

Lighting: The Friends of Rose Park community group has requested that lighting be considered for the entire length of the path. Currently, the middle segment of the trail, adjacent to the two playgrounds has medium scale, decorative pedestrian lighting, spaced at 75-ft intervals. Proposed lighting would match the same type, size, and style of the existing lighting.



The existing pedestrian lighting configuration in Rose Park is:

- Black contemporary pendant luminaires attached with Bishop's Crook arm and decorative scroll bracket;
- Mounted on black round tapered 12-ft poles;
- 75-ft +/- spacing

Note: Additional details such as light source, color temperature, distribution type, optics, wattage, photo control, etc. are not known at this time.

Rose Park

Proposed Improvements

Landscape plantings: Tree plantings are proposed along the trail to mitigate for tree removals elsewhere within the Rock Creek Park Multi-Use Trail project area. Proposed species include American Elm, Stately Manor Kentucky Coffeetree, Scarlet Oak, Fringe tree, and White Oak. A total of 32 new trees are proposed along the trail. Trees would be planted approximately 10-ft from the trail.

Fringe Tree



American Elm



White Oak

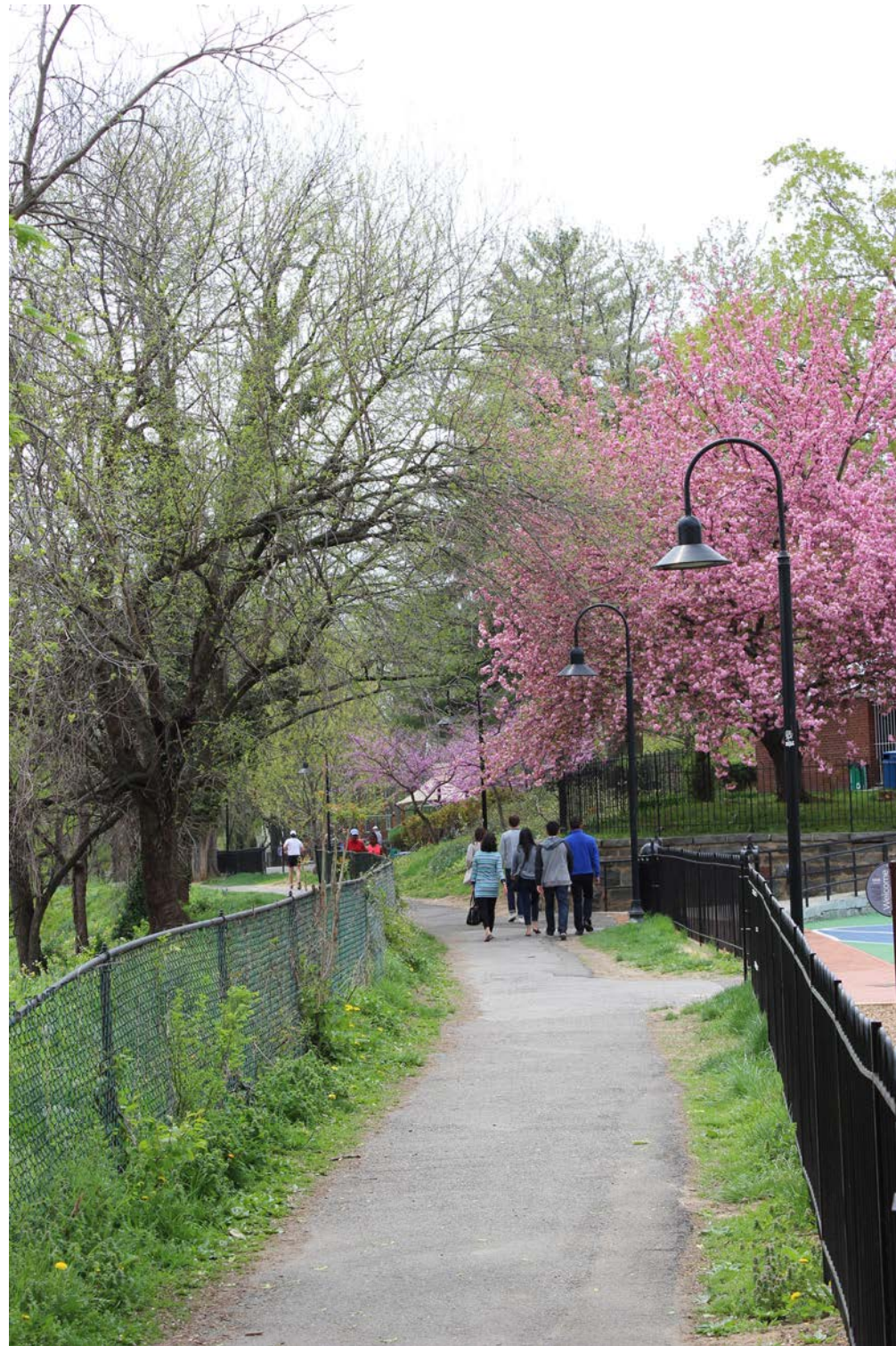


Kentucky Coffeetree



Scarlet Oak

Rose Park Trail—Existing and Proposed





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