



Reconstruction of
South Capitol Street Corridor
From I-295 Suitland Parkway Interchange to P Street
Segments 1 and 2

Concept Submission
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1.0 Executive Summary

This document presents the District Department of Transportation's (DDOT) preliminary design for Phase 1 of the South Capitol Street Project. Phase 1 encompasses the first two of five segments of the South Capitol Street Project considered by the Final Environmental Impact Statement (FEIS) signed in March 2011 as described below:

Segment 1: Replacement of the existing Frederick Douglass Memorial Bridge and transformation of the approach streets on both sides of the Anacostia River into a scenic boulevard befitting the southern corridor to the United States Capitol envisioned in Major Pierre L'Enfant's 1791 plan of the City of Washington.

Segment 2: Improvements to the existing I-295 interchange with Suitland Parkway by reconfiguring the ramps to remove traffic from Howard Road and connect directly with Suitland Parkway. The existing bridges carrying the I-295 over Howard Road, Suitland Parkway and Firth Stirling Avenue will also be replaced.

A second phase of the project will occur to construct the remaining segments, Segments 3, 4, and 5 as described below:

Segment 3: Modifications to Suitland Parkway at its intersection with Martin Luther King Jr. Avenue, including construction of a new diamond interchange.

Segment 4: Streetscape improvements to South Capitol Street between the Nationals Ballpark and the I-395, including construction of an at grade intersection with M

Street and re-configuration of the ramp bridges connecting South Capitol Street with the I-395.

Segment 5: Streetscape improvements to New Jersey Avenue.

Since the FEIS was signed, DDOT has advanced the design of the two segments that comprise phase 1 to further minimize right-of-way and environmental impacts for the Project and to increase the opportunities for community and pedestrian access to the crossing of the Anacostia River, and the connecting roads. This has been accomplished by modifying the alignment of the bridge crossing to be located parallel to the existing structure; and modifying the traffic circle, located on the eastern approach to the bridge, to an oval. This improves the compatibility of the geometric pattern of the roadways by mirroring the oval on each side of the river. The intent of this design is to recognize the prominence of this crossing as a gateway to the District.

The conceptual designs for the Frederick Douglass Memorial Bridge presented in this submission include options for both movable and fixed bridge types. In collaboration with the United States Coast Guard, DDOT is advancing a comprehensive navigational assessment for the Anacostia River at the location of the Frederick Douglass Memorial Bridge. At the conclusion of this assessment, DDOT will select a bridge type that will meet the reasonable needs of navigation to advance to final design and construction.

DDOT has initiated a Supplemental EIS process for all 5 segments of the Project as required by the National Environmental Policy Act (NEPA) to address the environmental benefits and impacts of the proposed revised design. This includes the navigational assessment and recommendation of fixed or movable bridge type.

DDOT will construct Phase 1 of the South Capitol Street project using the design build procurement method. The selection process for a qualified design build team commenced in June 2013 with the issue of a Request for Qualifications (RFQ) to interested teams. Following evaluation of the proposer teams Statements of Qualifications, DDOT plans to issue a Request for Proposals (RFP) to up to four shortlisted teams in the fourth quarter of 2013. The content of this Concept Submission will form an integral part of the technical requirements of the Request for Proposals. The requirements and guidelines contained herein will provide the framework for DDOT to evaluate the technical quality of each team's proposal and select a team based on a combined price and quality score.

1.1 Document Structure

This document is organized into the following sections:

- Section 1: Executive Summary - Provides a synopsis of the planning and design process, the projects goals and objectives, and the critical design considerations.
- Section 2: Introduction - Includes a description of the project history, project size, project cost, existing plan-

ning studies that have been considered in developing the preliminary design, a summary of the environmental documentation that has been prepared, agency and community coordination, and project implementation schedule.

- Section 3: Background Information - Presents information regarding regional, local and specific site context. This section also includes existing conditions information such as topography, site conditions, a brief history of Frederick Douglass, and a description of historic structures located within the project vicinity.
- Section 4: Analysis - Given the importance of the project to the District of Columbia, this section presents a detailed description of how the preliminary design has evolved to reinforce the geometry of the District as a whole and contributes to the development of the corridor.
- Section 5 : References - This section provides a list of planning documents, and design guidelines, standards and manuals used to produce this project.
- Appendix A: Requirements and Guidelines - This section presents specific visual design requirements and guidelines for the design-builder. This design criteria is required in order to enforce an acceptable level of aesthetics quality.

1.2 Goals and Objectives

Aesthetic consideration for any major project in Washington, DC must take into account how the design may affect the views of the city's major monuments. The Washington Monument will be plainly visible from the replacement Frederick Douglass Memorial Bridge both day and night, while the US Capitol dome will also be visible from certain points on the Bridge.

Additionally, the entire nearby Anacostia River waterfront is in flux. Contemporary developments such as the Nationals Stadium and the Yard's park alternate with empty parcels and declining industrial sites. It is reasonable to expect that within a decade the southeast DC waterfront will consist of numerous new or renovated buildings. These will be tied together by an active pedestrian promenade along both sides of the river's edge.

There is a large swath of park land on the eastern shore of the Anacostia River. An active path is planned along this side of the river as well, and will be tied into DC's bikeway/trail system. Also, along this southern stretch of South Capitol Street, the view will be of interchange infields and the Joint Base Anacostia-Bolling. The experience for southbound travelers will be of a transition from urbanity on the west bank to wooded greenery on the east bank, a suitable introduction to Anacostia and the suburbs beyond.

Given these factors, the following aesthetic goals were developed from the FEIS document and other studies, including those of the National Capitol Planning Commission. These goals shall be integrated throughout the design development of the proposed bridge:

Provide a monumental gateway to Washington DC

- Create an iconic bridge to complement existing Washington DC architecture and its surrounds.
- Strengthen the visual and physical gateway function of the bridge for drivers, bicyclists, pedestrians and viewers entering the monumental parts of the district as well as for those entering Anacostia.
- Similarly, recognize the gateway function of the bridge for river users entering or leaving the Anacostia River.

Maximize Connectivity

- Extend South Capitol Street to the Anacostia River.
- Embrace "The Plan of Washington" and maintain physical and visual connections of Q Street, R Street, and Potomac Avenue.

- Provide a multi-modal corridor that facilitates and encourages pedestrian, bike, and vehicular use by referencing current national and local development standards.
- Enhance multi-modal connections to existing and proposed adjacent land uses and developments.
- Provide pedestrian friendly at-grade intersections that are easily identifiable, and accessible.
- Restore, maintain and enhance views to and through the bridge for users of the riverside promenades and trails.

Encourage Economic Development

- Create a destination place for tourists.
- Support economic development in the corridor.
- Enhance opportunities for future development.
- Maintain future development opportunities from the west oval to South Capitol Street.
- Incorporate the 160 foot Build-To Line proposed by NCPD for future development along South Capitol Street.

Respect History

- Respect, reference and integrate the geometries, originally laid out by L'Enfant, that are still adhered to throughout the District.
- Protect the views of the Capitol Building
- Create opportunities for the placement of Memorials and Monuments.
- Respect existing historic places and features.
- Maintain Reservations 243, 244, 245 as public open spaces.
- Achieve the classical repose of previous Washington bridges but in a contemporary idiom suiting its surroundings.

Support Community

- Meet the aesthetic goals of the Anacostia Waterfront Initiative.
- Create public spaces for the local community to use and enjoy.
- Provide wide walks to introduce outdoor café's and public art.
- Avoid unnecessary land acquisitions and disturbances to adjacent properties.
- Create a pleasant and attractive environment for pedestrian and bike users of the bridge and its surrounds.

Be Sustainable

- Create a sustainable site that integrates current DDOT and DDOE Low Impact Development (LID) strategies into the design.
- Select plant and site materials that promote sustainable design.
- Use materials which are timeless in their appearance and durability.

1.3 Critical Design Considerations

The following is a summary of the critical design considerations that led to the design team's assumptions and recommendations.

- The FEIS considered a skew crossing of the Anacostia River. As part of the design development undertaken by DDOT since 2011, a revised parallel alignment adjacent and to the south of the existing bridge was adopted as the preferred alignment. Advantages of the re-alignment are as follows:
 - Avoids 5.2 acres of Navy property
 - Eliminates relocation of fuel pier
 - Minimize conflicts with 3 fiber optic/power cables
 - Easier to span Corps of Engineering (COE) levee
 - Shortens the river crossing
 - Eliminates relocation of west heliport
 - Potential savings of \$30-\$40 million.
 - The proposed Frederick Douglass Bridge and its required clearances for ships, grading, pier, and abutment locations dictated other proposed adjacent roads, walks, and wall elevations and appearance.
 - The DC Water Poplar Point Pump Station is considered to have historic significance and needs to remain. It is critical that proposed road locations and grading near the pump station blend with the existing grades and the local setting.
 - The Anacostia Pump House located in Poplar Point is outside of the project limits and is not affected by the design.
 - The proposed roadway design needs to stay south of the Sheridan Street Development development and provide connections to the overall pedestrian and bike circulation.
 - Minimize encroachment onto properties along Howard Road including Howard Road Academy (public charter school).
 - Right-of-ways (ROWs), topography, and vehicular, bike and pedestrian connections to the existing develop-
- ment were designed to integrate with the Barry Farm Improvements.
- Proposed roads, ramps, and walks were designed to connect with the Metro and Parking Garage.
 - The roadway and bridge geometry was designed to meet and blend with adjacent existing grading in the following areas:
 - National's Ballpark;
 - South Capitol Street;
 - U-Haul Moving & Storage;
 - Singh Transmission;
 - Auto-body Garage; and
 - DC Water (property between South Capitol Street to the east and Suitland Parkway to the west).
 - Reservations (#243, #244 and #245) should remain as open green spaces.
 - Disturbances to Anacostia Park, which is owned by the National Park Service (NPS), should be minimized for the reconfiguration of bridge ramps and roads.
 - Bike and pedestrian access to the Anacostia Riverfront Trail should be provided from both ends of the bridge.
 - The overall DC bike system can be enhanced by providing a smooth transition across the bridge.
 - A connection to the Suitland Trail (at Pomeroy Road) should be incorporated into the design.
 - NCPD future development plans show a main focal point or architecturally significant building at the terminus of South Capitol Street. The current design encourages this plan, and provides a site for this focal point. Although there is minimal disturbance to the adjacent property, right-of-way will need to be obtained, and retaining walls will need to be incorporated as a place holder for future development.
 - Proposed plans for Florida Rock were incorporated into the overall design. Walls will need to be constructed to manage topographical challenges. Pedestrian and bike connections to the property need to be maintained.
 - Several proposed plans for Poplar Point were reviewed and referenced when developing the east oval. Future connections to this property need to be maintained and shall respect L'Enfant's Principals as described in Section 3.1.
 - All proposed development must respect the 100-yr flood plain. The proposed development ultimately changes the flood plain location.
 - There are several areas with poor soils located within the Anacostia area. All foundations, paving, and Best

Management Practices (BMP's) must take this into consideration.

- In certain locations (primarily on the west side of the Frederick Douglass Memorial Bridge), minimal site disturbance is necessary due to contaminated soils. These contaminated sites will dictate portions of the stormwater management (SMW) strategies and locations.
- Several major utility lines including sanitary sewer, storm water, and combined sewers; water; electrical transmission; and natural gas—lie underneath the project area. From the O Street pumping station, large combined sewer siphons extend south underneath the Anacostia River, just east of the Frederick Douglass Memorial Bridge.
- PEPCO owns several underground power transmission lines that extend from the Buzzard Point power plant. One line extends north under South Capitol Street. Other lines extend across the Anacostia River just south of the existing Frederick Douglass Memorial Bridge and continue south along South Capitol Street and east along Suitland Parkway.
- Enhanced bio-retention and open bio-retention areas should be incorporated into the project to meet DDOT and DDOE SWM requirements.
- ROW disturbance was kept to a minimum with some partial takes and some overall takes of adjacent parcels.

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2.0 Introduction

2.1 Project Name

Reconstruction of the South Capitol Street Corridor - Segments 1 and 2.

2.2 Agencies

Submitting Agency:

District of Columbia Department of Transportation (DDOT)
55 M Street SE, NO 400
Washington, DC 20003

Agency Program Manager:

Sanjay Kumar, P.E.
AWI Program Manager
202.741.5356
sanjay.kumar@dc.gov

2.3 Description of the Project

This project is a comprehensive project consisting of two phases. This document is for Phase 1, Segments 1 and 2 (Figure 2-2).

The South Capitol Street Corridor Project calls for replacing the Frederick Douglass Memorial Bridge and transforming the bridge and the approaching streets on both sides of the bridge into a grand scenic boulevard that improves safety and increases pedestrian and vehicular safety, improves multi-modal transportation options, increases community accessibility and supports economic development on both sides of the Anacostia River.

Constructed in 1949, and dedicated to Frederick Douglass in October 1964, the existing bridge carries South Capitol Street over the Anacostia River. South Capitol Street was a primary corridor in Major Pierre L'Enfant's 1791 Plan of the City of Washington and was envisioned as one of the symbolic gateways to the city and its Monumental Core. As envisioned in L'Enfant's Plan, South, East, and North Capitol streets represent the cardinal directions extending from the U.S. Capitol. As such, these streets are critically important within the hierarchy of Washington's streets and are considered to be "prominent gateways" to the Monumental Core.

The bridge is located at the southeast quadrant of the District of Columbia and immediately northeast of the confluence of the Potomac and Anacostia Rivers. The Washington Navy Yard is directly upstream from the existing bridge. The ballpark for the Washington Nationals Major League Baseball team is directly north of the bridge. Southeast of the bridge is the U.S. Navel Support Facility Anacostia, and northeast of the bridge is Poplar Point.

As part of the South Capitol Street Corridor the bridge provides a key component of the road network linking downtown Washington to neighborhoods in the southeast and southwest quadrants of the District of Columbia and Prince George's County, Maryland. In addition to local traffic the corridor provides direct connectivity between the SE-SW Freeway, I-295, I-395, and Suitland Parkway. Due to its proximity to the U.S. Capitol, South Capitol Street has also been designated as part of the 61,000-mile

Strategic Highway Network by the U.S. Department of Defense and FHWA. The importance of this corridor as an evacuation and emergency route was heightened after September 11, 2001, because it provides a protected route for movement of security resources and our nation's leaders to and from military installations at Andrews and Bolling Air Force bases and the Washington Navy Yard.

Most of the areas north and west of the river are currently used for business, commercial and recreational purposes, although there are residential neighborhoods in the project area, and redevelopment of industrial properties to mixed use is rapidly occurring. The Anacostia River is typically used by small craft; and provides access to the Navy Yards, currently served by smaller vessels.

While preliminary engineering and right-of-way land acquisition for the project have been underway since January 2012, final federal environmental approval is still pending.

Key Project Elements

- Building a new six-lane Frederick Douglass Memorial Bridge including an 18' wide pedestrian/bike path on both sides of the bridge;
- Creating a new traffic oval west of the river that connects South Capitol Street, Potomac Avenue, and Q and R Streets SW;
- Creating a new at-grade traffic oval east of the river that connects South Capitol Street, Suitland Parkway and Howard Road SE;

- Reconstructing the Suitland Parkway/Interstate 295 interchange;
- Reconstructing I-295 Bridges over Howard Road, Suitland Parkway, and Firth Sterling Avenue;
- Increasing bicycle and pedestrian facilities; and
- Improving drainage and storm water management throughout the corridor.

2.4 Site Area

Segment 1	34.55 acres
Segment 2	29.48 acres
Total	64.03 acres

2.5 Project Planning Process

The purpose of the South Capitol Street project is to improve safety, multimodal mobility, and accessibility, and support economic development throughout the project area.

Extensive planning efforts have been underway for more than a decade to transform South Capitol Street into an urban boulevard that responds to and serves its local context while restoring its function as a symbolic gateway. Figure 2-1 shows the planning projects or actions specifically related to South Capitol Street. In 1997, the National Capital Planning Commission (NCPC) completed a framework plan, *Extending the Legacy: Planning America's Capital for the 21st Century* (NCPC 1997) (*Extending the Legacy Plan*), that extended the Monumental Core to include South Capitol Street. In its plan, NCPC envisioned "South Capitol Street as a bustling mix of federal, local, and private uses. A major memorial or public building can be located on the important site where the street meets the river, and the new South Capitol Street can become a lively area of shops, restaurants, housing, offices, and open space."

NCPC's *Memorials and Museums Master Plan* (2001) built on the 1997 *Extending the Legacy Plan* by defining a long-range plan for new museums, memorials, and other public buildings, strategically distributed to all quadrants of the city. The 2001 master plan identified 100 potential sites for future memorials and museums, including the South Capitol Street terminus at the Anacostia River, the Southeast-Southwest (SE-SW) Freeway, and the south shore of the Anacostia River in Anacostia Park, SE as prime sites. In the plan, prime sites were defined as those of the highest order. Because of high visibility and strong axial relationships with the U.S. Capitol and the White House, the plan proposed that these sites be reserved for subjects of lasting historical and national importance. NCPC continued planning work for the South Capitol Street Corridor with the *South Capitol Street Urban Design Study* (NCPC 2003).

Recognizing the need for a clear vision for the Anacostia Waterfront, including the South Capitol Street Corridor, former Mayor Anthony Williams brought together the 20 federal and District of Columbia agencies that own or control land along the Anacostia River in March 2000. The outcome of this partnership was the *Anacostia Waterfront Initiative (AWI) Memorandum of Understanding (MOU)*. Acting with the cooperation and oversight of this partnership, the District of Columbia Office of Planning (OP) created the *AWI Framework Plan*

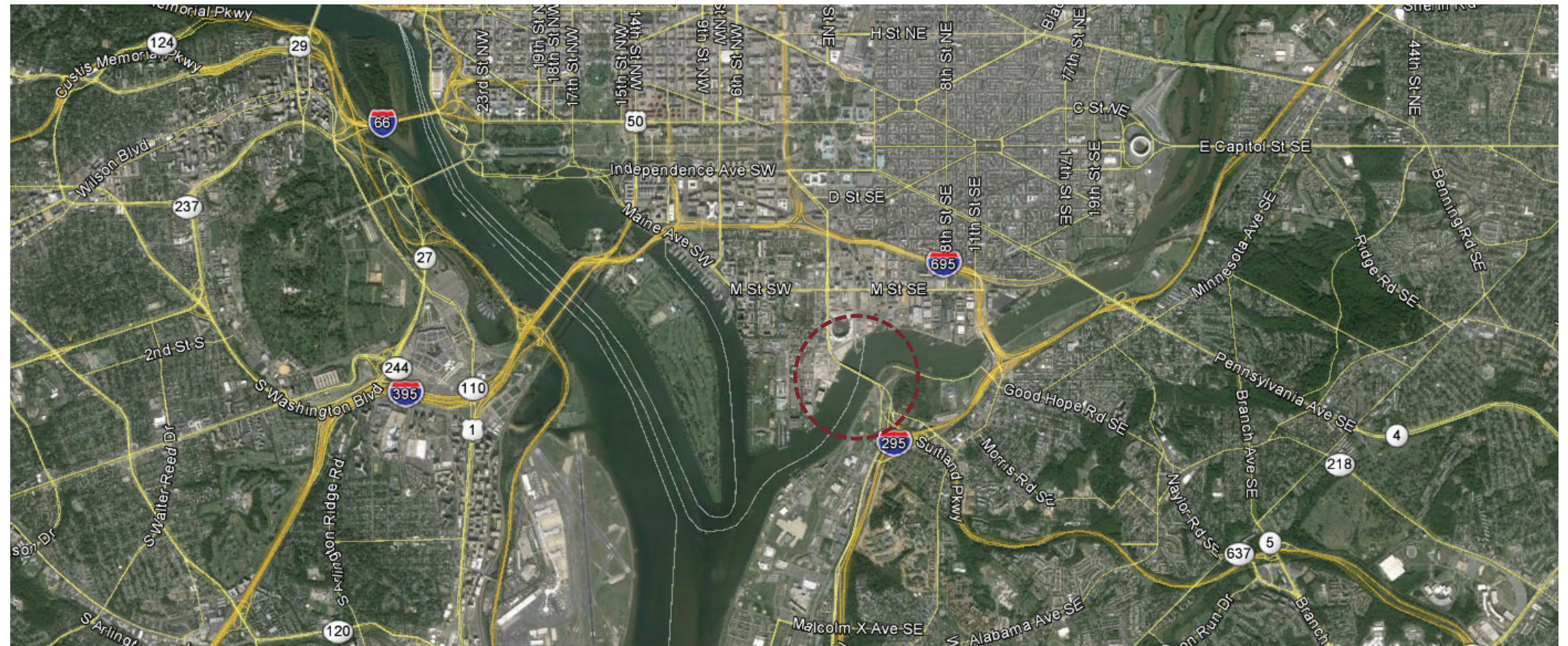


Image 2-1 Existing bridge location - Regional context



Image 2-2 Existing Frederick Douglass Memorial Bridge

As defined in the AWI Framework Plan (OP 2003), the future of the Anacostia Waterfront will include a cleaner river, strong waterfront neighborhoods, new and revitalized waterfront parks, and vibrant cultural attractions, as well as better access both to and between these neighborhoods and destinations. In 2002 and concurrent with OP's AWI Framework Plan, the District of Columbia Department of Transportation (DDOT) began planning for the corridor by undertaking a series of planning and engineering studies to investigate and analyze the Anacostia Waterfront's existing and future transportation patterns, needs, opportunities, and constraints. These efforts led to the completion of several transportation planning studies, including The South Capitol Gateway and Corridor Improvement Study (DDOT 2003) (Gateway Study), South Capitol Gateway Corridor and Anacostia Access Studies (DDOT 2004), a tunnel study to determine the feasibility to replace the SE-SW Freeway, the South Capitol Street Bridge Design Workshop (March 30 and 31, 2005), and the Frederick Douglass Memorial Bridge Alignment Study (DDOT 2007) (Bridge Alignment Study).

The Gateway Study (DDOT 2003) created a vision for transforming South Capitol Street from an unsightly freeway into a grand urban gateway. Performed at the direction of the U.S. Congress, the Gateway Study (DDOT 2003) was "a study of methods to make improvements to promote commercial, recreational, and residential activities and to improve pedestrian and vehicular access on South Capitol Street and the Frederick Douglass Memorial Bridge." The Gateway Study (DDOT 2003) proposed that South Capitol Street become a gracious urban boulevard consistent with the past goals defined in the L'Enfant and Macmillan Commission plans, which would accommodate bicycles, pedestrians, and transit vehicles, as well as automobiles and commerce. The study also recommended construction of a new Frederick Douglass Memorial Bridge on a southern alignment and at a more urban scale than the present bridge. The transportation improvements would also improve access to new activity centers and support economic development and revitalization of the Anacostia Waterfront.

The recommendations from the Gateway Study (DDOT 2003) were examined in more detail in the South Capitol Gateway Corridor and Anacostia Access studies. The study findings for the South Capitol Street Corridor were as follows:

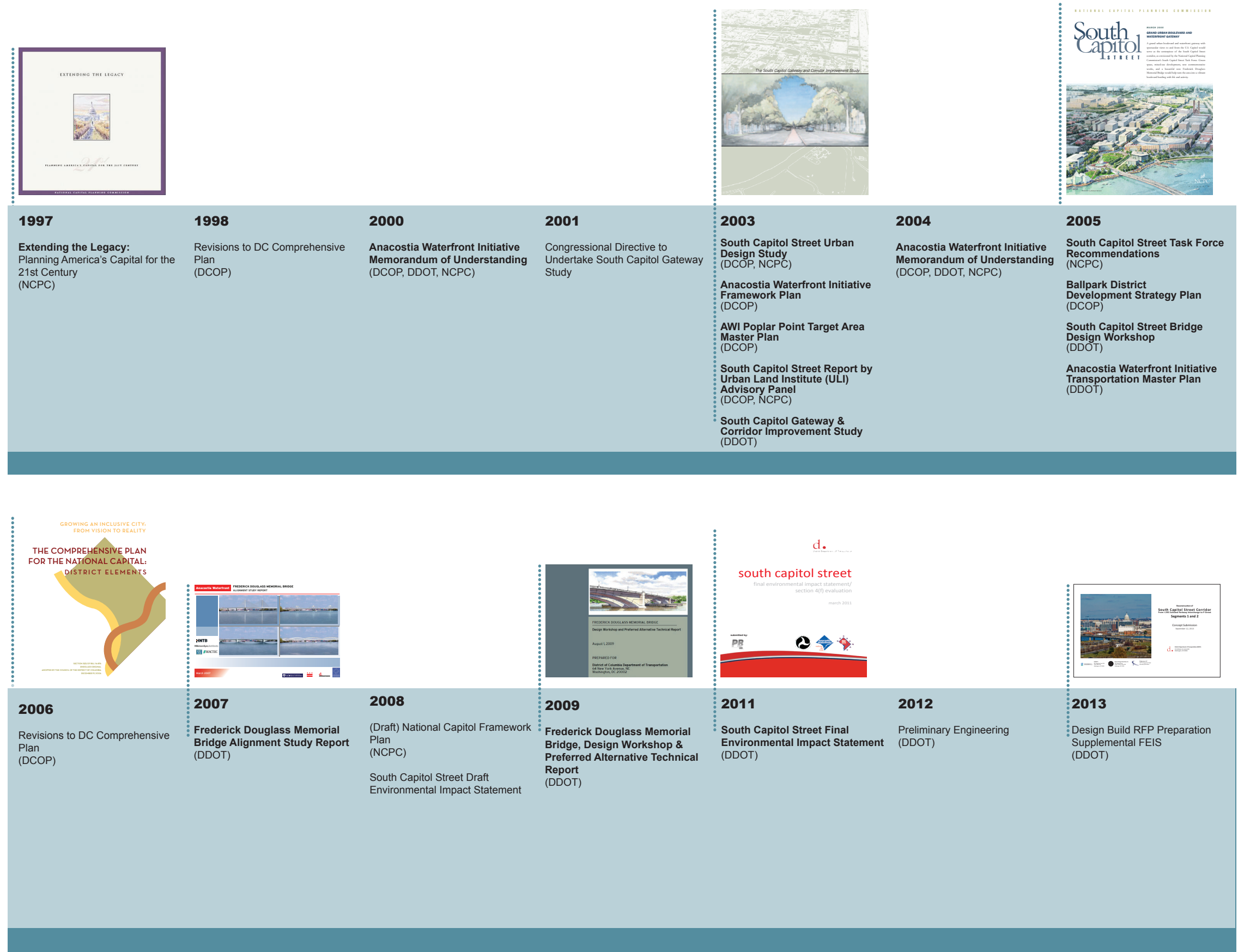


Figure 2-1 South Capitol Street Planning Efforts

- South Capitol Street should be configured as an impressive urban gateway to the U.S. Capitol and the Monumental Core that accommodates various forms of transportation (automobiles, transit, pedestrians, and bicyclists). (DDOT 2004).
- Change is rapidly occurring in both the corridor and the Anacostia Waterfront area; therefore, transportation and infrastructure improvements should be initiated and implemented to keep pace with economic development initiatives and the needs of residents and businesses.
- The Frederick Douglass Memorial Bridge is in need of replacement; delays in the process will require more expensive repairs and perhaps restrictions of bridge traffic. The deficiencies that pose safety issues include local roads serving regional traffic, weaving traffic patterns, non-standard pedestrian and bicycle facilities, and lack of crosswalks or pedestrian signals, and median barriers and grade separations.
- The South Capitol Street/I-295/Suitland Parkway interchange is inefficient and does not provide direct access from I-295 southbound to South Capitol Street northbound.
- Transportation system continuity should be maintained. South Capitol Street provides local and regional access as well as access to key military installations in the region, and is the main route used by the President and national dignitaries to visit the Capital.
- Bicycle and pedestrian access throughout the project area is inconsistent and substandard.

As the project developed momentum, the Anacostia Crossings Project Memorandum of Understanding (2004) clarified the federal and local partnership. As explained in the document, the U.S. Department of Transportation (USDOT) would commit technical resources to expedite environmental and project development phases. At the same time, the District of Columbia would create, through ongoing communication and collaboration, a climate among local interests and constituencies that would help the project advance.

FHWA and DDOT completed the Draft Environmental Impact Statement/Section 4(f) Evaluation (DEIS) in February 2008 and held Public Hearings in March 2008 to obtain public and agency comments on the findings contained within the DEIS. Two Build Alternatives and four alternative bridge types were evaluated in the DEIS. Following the review of comments on the DEIS

Alternatives, DDOT developed another alternative as part of the Final Environmental Impact Statement (FEIS), which was completed in March of 2011.

The FEIS Preferred Alternative includes refinements developed in response to public and agency comments. Public involvement and coordination efforts with both federal and District regulatory review agencies, key stakeholders, and the public continued through development of the FEIS Preferred Alternative.

2.6 Procurement Strategy

DDOT will construct these segments of the South Capitol Street project using the design build procurement method. A two step process will be adopted to first pre-qualify and then select the design build team.

The first step commenced in June 2013 with the issue of the request for qualifications (RFQ). This asked interested teams to submit statements of qualification (SOQ) based around a detailed set of submittal requirements that will allow DDOT to evaluate proposer teams. This included the team's past experience with projects of similar scope and complexity and qualification of key personnel that includes both a lead urban designer and lead bridge architect. DDOT will evaluate each SOQ to shortlist up to the four most qualified design build teams who will be invited to participate in the second step of the process.

For the second step, in the fourth quarter of 2013 DDOT will issue a Request for Proposal (RFP) document to each of the shortlisted teams. The RFP will set forth the minimum design requirements and constraints for the project developed by DDOT through its preliminary design studies. This concept submission is a part of the RFP and will set the standards for visual quality through the requirements and guidelines contained herein. Teams are required to develop a detailed technical and price proposal responsive to the RFP document.

Given the importance of this project to both the local community and the wider region, DDOT will evaluate proposals based on a combined score for price and technical quality. To allow an effective review of each team's technical proposal DDOT has developed comprehensive technical proposal submittal requirements that include components tailored specifically towards aesthetics, including:

- A narrative description of the proposals aesthetic concept, describing how it addresses and exceeds the visual quality requirements contained in the RFP. This

shall include the bridge, its approaches, landscaping concept and aesthetic lighting concept.

- Drawings showing the engineering concept for the bridge developed to sufficient detail to allow a review of the architectural details proposed including barriers, railings and lighting.
- Conceptual landscaping scheme.
- Renderings of the bridge from various defined viewpoints, including night renderings to show the aesthetic lighting concept.

The score for technical quality will be developed through a detailed assessment of these and other parts of the design builder's technical proposal.

DDOT will complete the procurement process with the selection of a design build team around the second quarter 2014, and construction to commence at the beginning of 2015. It is envisaged that construction will be completed during 2018. A more detailed schedule is included in section 2.14 of this submission.

2.7 Assigned Employment

The purpose of the proposed action is to replace the deteriorated bridge, and provide a gateway that supports economic development in the project area. The Anacostia Waterfront, including the South Capitol Street Corridor, has recently experienced and will continue to experience substantial growth as planned for and promoted by numerous federal and District entities.

The need to support economic growth is demonstrated by the density of employment and residential development forecasted for the area. Public-driven development, including the USDOT Headquarters, Washington Navy Yard, Nationals Park, Poplar Point, Barry Farm, and St. Elizabeth Hospital, will add thousands of jobs and create new residential neighborhoods. This public investment has encouraged private investment in large-scale office and residential buildings. The influx of new employees, residents, and visitors to the project area will increase the demand on the transportation system. The forecasted population and employment growth in the South Capitol Street Corridor demonstrates the need for a multimodal transportation system that connects residential communities, employment and activity centers to the region. As shown in Table 1-2, the Metropolitan Washington Council of Governments (MWCOCG) forecasts that the South Capitol Street project area will grow at a much higher rate than the rest of the District of Columbia and the region. By 2030, the project area's

population is expected to increase almost 101% percent and employment is expected to increase by approximately 100 percent. This growth is substantial, particularly when compared to anticipated District and regional trends.

2.8 Project Estimate

Location	Employment (in thousands)		Percent Change
	2000	2030	
South Capitol Street Project Area	47.6	95.7	101%
Washington, DC	743.6	922.4	24%
Washington Regional area	3506.7	5272.3	50%

To best manage financing costs and potential construction impacts, DDOT has divided the project into five segments that are to be designed and constructed in two phases.

Phase 1 includes Segments 1 and 2, and is estimated to cost approximately \$600 million. Segment 1 consists of replacing the existing Frederick Douglass Memorial Bridge with new ovals. Segment 2 consists of constructing a new Suitland Parkway/I-295 Interchange.

Phase 2 includes the three remaining segments that are to be advanced in future years at total cost of approximately \$300 million.

LEGEND

Phase 1

- Segment 1** - Frederick Douglass Memorial Bridge and Approaches
- Segment 2** - I-295/Suitland Parkway Interchange Reconstruction

Phase 2

- Segment 3** - New Interchange at Suitland Parkway/Martin Luther King, Jr. Avenue, SE
- Segment 4** - South Capitol Street Including M Street
- Segment 5** - New Jersey Avenue Streetscape Improvements

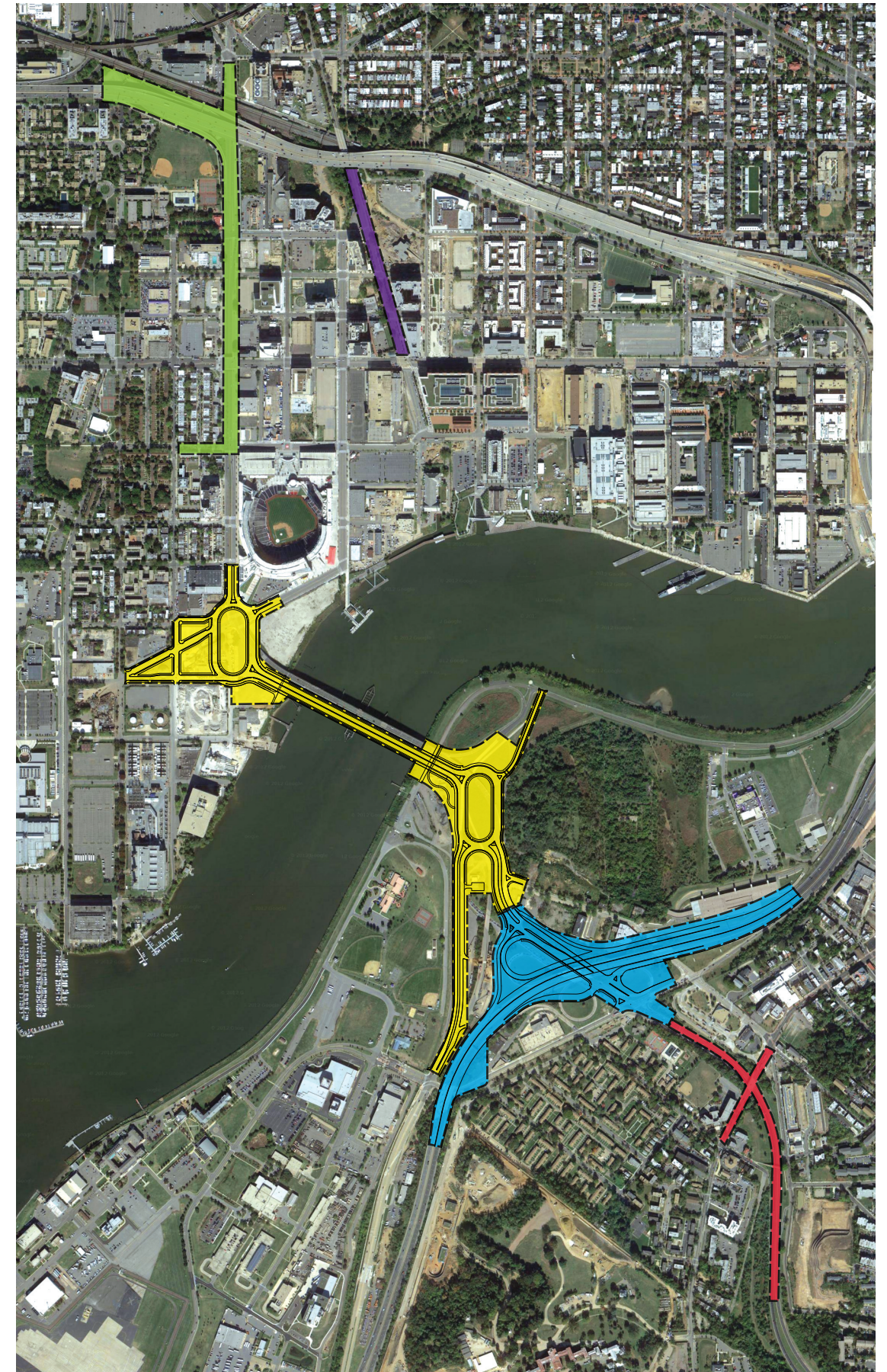


Figure 2-2 Project Segments

2.9 Environmental Documentation

The Final Environmental Impact Statement (FEIS) for the project was completed in March of 2011. Since that time, revisions to the project design have occurred and have warranted an additional environmental review. The benefit in the design revisions is that many of the prior environmental and right-of-way impacts have been eliminated or reduced.

In July 2013, FHWA and DDOT initiated a Supplemental EIS process to evaluate the environmental benefits and impacts of the revised design of the project. The Draft Supplemental EIS will be prepared in the fall of 2013 and is planned to be approved in early 2014. In addition to the preparation of the Supplemental EIS, consultation pursuant to Section 106 of the National Historic Preservation Act will be conducted and the current Memorandum of Agreement (MOA) for the project is expected to be revised.

Public involvement activities are also being planned concurrent with the environmental process in 2013 and 2014. This includes the distribution of updated information on the project website, the distribution of newsletters, and conducting public meetings to update the public on the status of the project, the revised design, and the resulting benefits to the community.

2.10 Floodplains Management and Wetlands Protection

The National Flood Insurance Program defines 100-year floodplains as “areas that will be inundated by the flood event having a one percent chance of being equaled or exceeded in any given year.” Executive Order 11988 (Floodplain Management) and 23 CFR 650.11 require that federal actions, to the extent possible, avoid short- and long-term impacts to floodplains and avoid direct or indirect support of floodplain development where a practicable alternative exists. The DDOE Watershed Protection Division reviews all projects proposed within the District of Columbia floodplains as part of the District of Columbia Floodplain Management Program to ensure that development is consistent with the need to minimize or eliminate flood damage. The District of Columbia’s program also coordinates most of its activities with the Federal Emergency Management Agency’s (FEMA) National Flood Insurance Program and the USACE. The 100-year floodplains were located within the project area using the FEMA Flood Insurance Rate Maps for the District of Columbia.

The project area crosses the 100-year floodplain of the Anacostia River. East of the Anacostia River the floodplain is largely contained by the river bank, with the exception of an area north of the Frederick Douglass Memorial Bridge that includes the NPS administration buildings and a portion of Poplar Point, as shown in Figure 3-10. West of the Anacostia River, the floodplain extends outside of the banks an average of 200 feet, with some areas reaching inward 200 to 450 feet along low-lying areas. A majority of the floodplain in the project area has been modified to accommodate the development and urbanization of the lower Anacostia River. Two flood-control devices are located in the project area on the east bank of the Anacostia River along Poplar Point (see Figure 3-10). A levee extends from Bolling Air Force Base and Anacostia Naval Station to the tip of Poplar Point. Inside the military bases this levee consists primarily of an earthen berm upon which is a paved multi-use trail. Outside the fence of the Anacostia Naval Station, the levee continues upstream as an earthen berm without a trail, ending near the Poplar Point pump station. Upstream of the levee, a flood wall starts at the point where the higher Poplar Point ground meets the lower ground near the NPS park headquarters facility in Anacostia Park. The remainder of the levee is comprised of steel pilings. The levee is maintained by public agencies, including the NPS and the U.S. Navy.

Wetlands are jointly defined by the USEPA, the USACE, and the District of Columbia as “those areas that are inundated or saturated by surface or groundwater at a frequency and duration sufficient to support, and that under normal circumstances do support, a prevalence of vegetation typically adapted to life in saturated soil conditions” (USEPA, 40 CFR 230.3 and USACE, 33 CFR 328.3; DC Law 5-188; DC Official Code §§ 8-103.04 and 8-103.20). Wetlands that are connected hydrologically to other waterways are regulated by the USACE, while wetlands isolated from other wetlands or waterways are regulated by the USEPA. Wetland areas were located using USFWS National Wetland Inventory maps, the USGS maps for the Anacostia and Alexandria Quadrangles, and the NRCS Soil Survey of District of Columbia (1975). Field investigations were conducted on December 9, 2004 and February 2 and 22, 2005 in areas where USFWS, USGS, and NRCS mapping indicated the presence of wetlands or hydric soils. Wetland determinations and delineations were conducted during the field surveys. Wetlands were identified in accordance with the 1987 Corps of Engineers Wetland Delineation Manual (USACE 1987). This approach

is based on three parameters: hydrology, hydric soils, and hydrophilic vegetation. Soil color was identified using a Munsell Soil Color Chart (2000). The wetland indicator status of the observed vegetation was identified using the National List of Plant Species that Occur in Wetlands: Region 1 – Northeast (USFWS 1988). Wetland functions and values were evaluated in the field using best professional judgment. Wetland boundaries were then marked in the field and mapped with the use of a global positioning system (GPS). Six wetlands, totaling 6.9 acres, were identified within the South Capitol Street project area (see Figure 3-10). These include four palustrine emergent wetlands, one forested wetland, and one combination wetland with forested, palustrine emergent and scrub shrub vegetation. The majority of wetlands are located within Anacostia Park on Poplar Point. Four wetlands are isolated or non-jurisdictional because of their lack of hydrologic connectivity to other streams and wetlands. Two wetlands are considered jurisdictional. In April 2005, representatives from the USACE, NPS, DDOE, and DDOT conducted field verifications of the wetlands and waterways identified and their boundaries. A letter from the USACE dated July 1, 2005, provided an official “Jurisdictional Determination” of all wetlands and waterways regulated by the USACE.

2.11 Historic Preservation Documentation

No federally recognized tribal governments exist or are located in the District of Columbia; therefore there are no formally indigenous tribal governments to contact. Tribes indigenous to this area were assimilated into other tribes or victimized by disease by the early 18th century. Sites, buildings, structures, and objects that could be affected by a proposed undertaking are identified and evaluated in either architectural or archaeological investigations. They are often referred to under the blanket term “cultural resources” and sometimes just as “properties.” Some cultural resources are historic and others are not. The term “historic” is used for those properties that are significant in the fields of history, architecture, engineering, archaeology, or culture. The National Register of Historic Places (NRHP) was established as the nation’s repository of documentation for those historic properties. The NPS developed the following guidelines for the selection of properties to be included in the NRHP in a manner that is consistent with the Secretary of Interior’s Standards and Guidelines for Archaeology and Historic Preservation:

- Criterion A – association with events that have made a significant contribution to the broad patterns of our history; (NPS 1983). A building, site, structure, or object is significant, i.e., historic, if it possesses integrity of location, design, setting, materials, workmanship, feeling, and association and fulfills at least one of the following National Register Criteria of Evaluation:
- Criterion B – association with the lives of persons significant in our past;
- Criterion C – embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values, or represents a significant and distinguished entity whose components may lack individual distinction; and
- Criterion D – has yielded or may be likely to yield information important in prehistory or history (36 CFR Part 60.4).

Some categories of properties are not ordinarily considered eligible for the NRHP, i.e., cemeteries, birthplaces or graves of historical figures, properties owned by religious institutions or used for religious purposes, structures that have been moved from their original locations, reconstructed historic buildings, properties primarily commemorative in nature, and properties that have achieved significance within the

past 50 years (36 CFR Part 60.4). These properties may be eligible only under special circumstances called criteria considerations. It was found that the only criteria consideration germane to this investigation is Criteria Consideration A: a religious property deriving primary significance from architectural or artistic distinction or historic importance. That property is St. Vincent de Paul Catholic Church, located at the northeast corner of South Capitol Street and M Street SE, and found eligible for its distinctive architecture. The National Historic Preservation Act of 1966 requires the consideration of the potential impacts of federally funded projects on cultural resources that are listed in the NRHP or on properties found eligible for the NRHP, even if not actually listed.

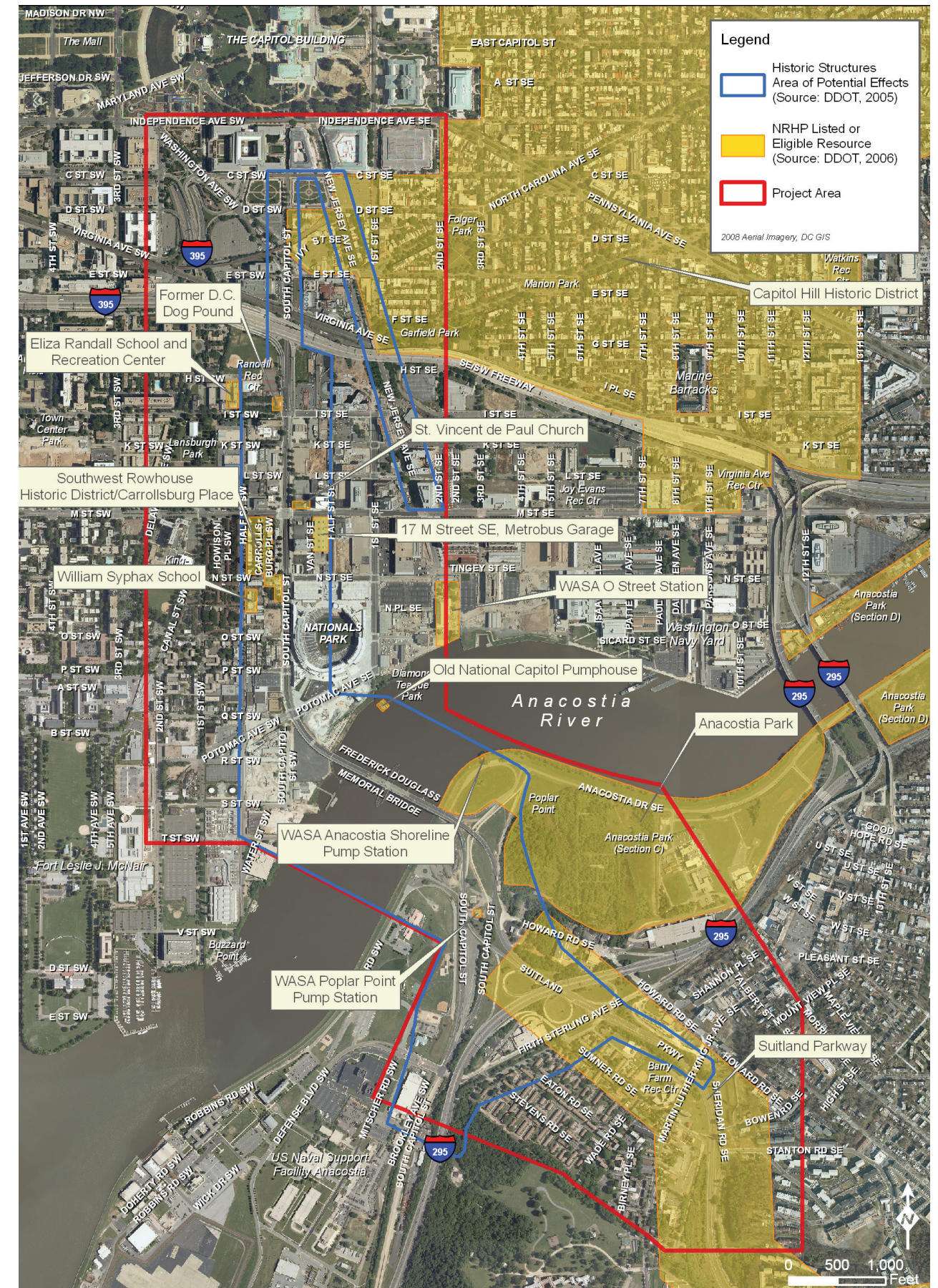


Figure 2-3 National Register of Historic Places Listed and Eligible Properties

2.12 Project Coordination

2.12.1 Recent Agency Design Coordination Meetings

The ultimate goal of the project is to avoid, minimize and/or mitigate adverse effects on the Plan of the City of Washington.

As a group, DDOT consulted with National Capitol Planning Commission (NCPC), the United States Commission of Fine Arts (CFA), and the State Historic Preservation Office (SHPO) and the National Park Service (NPS) on the bridge and streetscape improvements along the South Capitol Street Corridor Segments 1 and 2. Process and progress meetings were held on:

- September 12, 2012
- October 18, 2012
- November 13, 2012
- January 23, 2013
- February 12, 2013
- July 31, 2013
- August 8, 2013

During each meeting, DDOT presented updates to the project, including design, schedule, and cost. Comments from each of the key stakeholders were recorded and incorporated into the design.

2.12.2 Stakeholder Coordination

An inter-agency Committee was formed in September 2002 as part of the ongoing planning efforts for the Gateway Study, which was the first planning effort that was initiated as part of the AWI program. The inter-agency Committee consists of representatives from numerous regulatory, resource, and local government agencies as listed below:

- Anacostia Waterfront Corporation (AWC)
- Architect of the Capitol
- Bolling Air Force Base
- District of Columbia Commission on the Arts and Humanities
- District of Columbia Department of Housing and Community Development
- District of Columbia Department of Parks and Recreation (DPR)
- District of Columbia Department of Property Management
- District of Columbia Department of the Environment (DDOE) — Water Quality Division

- District of Columbia Historic Preservation Office (DC SHPO)
- District of Columbia Office of Planning (OP)
- District of Columbia Office of the Deputy Mayor for Planning and Economic Development
- District of Columbia Water and Sewer Authority (DC Water)
- Ft. Myer/National Defense University
- Maryland State Highway Administration
- Metropolitan Washington Council of Governments (MWCOG)
- National Capital Planning Commission (NCPC)
- National Marine Fisheries Service (NMFS)
- National Park Service (NPS)
- U.S. Army Corps of Engineers (USACE)
- U.S. Coast Guard (USCG)
- U.S. Environmental Protection Agency (USEPA)
- U.S. Fish and Wildlife Service (USFWS)
- U.S. General Services Administration (GSA)
- U.S. Navy
- Washington Metropolitan Transit Authority (WMATA)

From its inception in September 2002 through February 2008, the committee met on the second Tuesday of every month to discuss related projects and to review and provide comments on the South Capitol Street project. In January 2009, the inter-agency coordination meetings were reinitiated for all AWI projects by DDOT.

2.12.3 Community Participation

Since the signing of the March 2000 Anacostia Waterfront Initiative (AWI) Memorandum of Understanding and the public release of the AWI Framework Plan (District of Columbia Office of Planning 2003), there have been more than 30 public involvement activities related to the AWI. Many of these activities have included, either directly or indirectly, discussion and opportunities to provide input regarding the formation of the South Capitol Street project and its scope.

In 2002, the District of Columbia Department of transportation (DDOT) initiated a series of studies to research and evaluate potential improvements to the South Capitol Street Corridor. The first of these studies was the South Capitol Gateway and Corridor Improvement Study (DDOT 2003)(Gateway Study). The Gateway Study was completed under a Congressional mandate to look at improvements to South Capitol Street due to its

importance and as a main entry point to the nation's Monumental Core. Recommendations from this study included refining transportation solutions with the Anacostia Access Study, Bridge Alignment Study, Tunnel Study, and Environmental Impact Statement. The South Capitol Gateway Corridor and Anacostia Access Studies (DDOT 2004) recommended near-, mid-, and long-term transportation improvement projects. The long-term improvements are included in the South Capitol Street project. In an effort to gain community input and feedback on proposed improvements specific to the South Capitol Street Corridor, DDOT also initiated a public involvement program in 2002. This public involvement program was developed to encourage information sharing throughout both earlier studies, which preceded the South Capitol Street project.

Community briefings and presentations about the South Capitol Street project have been made at the request of various local groups and organizations. These briefings and presentations were typically given at regularly scheduled meetings for the interested groups. During the DEIS, briefings and presentations were made to the following groups:

- ANC 6D (December 2005)
- NC 8A (December 2005)
- Capitol Hill Senators Briefings (October 2004, February 2005, and January 2006)
- District of Columbia – Bicycle Advisory Council Meeting (March 2006)
- District of Columbia – Sports and Entertainment Commission (bi-weekly meetings held between April 2005 and April 2008)
- Florida Rock (January 2006)
- Friends of Garfield Park (January 2007)
- Poplar Point (October 2006, November 2006, and January 2007)
- Sierra Club (January and September 2006)
- Southwest Neighborhood Assembly (January 2006)
- William C. Smith Company (June 2007)

During the FEIS, briefings and presentations were made to the following groups:

- William C. Smith Company (May 2008)
- Florida Rock Properties (September 2008)
- ANC 6B (January 13, 2009)
- ANC 6D (February 2, 2009)
- ANC 8A (February 3, 2009)
- ANC 8C (January 7, 2009)

Public Hearings for the South Capitol Street DEIS/Section 4(f) Evaluation were held on March 4, 2008, at Birney Elementary School, 2501 Martin Luther King, Jr. Avenue SE; and on March 5, 2008, at Amidon Elementary School, 401 I Street SW. The hearings, which were held from 6:30 p.m. to 8:00 p.m., were held to present the findings of the DEIS and to provide the public with an opportunity to comment on the alternatives and findings of the DEIS. These meetings were advertised in local newspapers, via e-mail announcements, and in the February 2008 project newsletter.

The information presented and the format of the public hearings were the same at both locations. An open house format began each meeting, at which time the public was able to review project displays and discuss the information with project staff. The informational displays illustrated the purpose of and need for the project, the alternatives development process, bridge design options, Build Alternatives 1 and 2, right-of-way requirements, and impacts on sensitive resources affected by the project. Because the project could adversely affect historic resources, information was specifically presented on the Section 106 (cultural resources) investigations completed for the project.

A formal presentation about the South Capitol Street project was given by DDOT, which was followed by a public testimony session. A court reporter was available to record oral comments, both during the testimony session and privately. Attendees were encouraged to provide comments and suggestions by the following means:

- Providing testimony at the Public Hearings
- Filling out the comment forms provided at the Public Hearings
- Mailing written comments to: Barton Clark, PE, DDOT Project Manager, DDOT-IPMA 64
- New York Avenue NE, Washington, DC 20002
- Faxing comments to: (202) 671-4710
- E-mailing comments to: comments@southcapitoleis.com
- Submitting comments via a link provided on the project website: www.southcapitoleis.com

During the 45-day comment period, which ended on March 31, 2008, the Federal Highway Administration (FHWA) and DDOT received comments, letters, faxes, and e-mails from agencies, organizations, and concerned citizens. Many of the comments focused on specific design features of the Build Alternatives and others focused on general project issues.

Comments also were received from 18 nongovernmental organizations:

- ANC 8A
- ANC 8C
- Anacostia Community Boathouse Association
- BN Temple Community Development Corporation
- Far SW-SE CDC
- Georgetown University Law Center Institute for Public Representation
- Howard Road Neighborhood Corporation
- Main Street Anacostia
- The Overlook at Washington View Condo Association
- Oxford Manor Tenant Association
- Poplar Point One, South, North, and East, LLC
- Potomac Riverboat Company
- Sales Place Homes
- Sheridan Terrace Redevelopment, LLC
- Southwest Neighborhood Assembly District 4
- Town of Hillside Homeowners Association
- United Black Funds
- Washington View Tenant Organization

A total of 27 citizens provided comments, which addressed a wide range of topics and concerns. The highest volume of public comments addressed the closure of Sheridan Road at Martin Luther King, Jr. Avenue and Howard Road, which was proposed under Build Alternatives 1 and 2.

Public comments focused on the following topics:

- Opposition to closing Sheridan Road at Martin Luther King Jr. Avenue and Howard Road due to public safety, crime, traffic, accessibility, and economic development concerns;
- Opposition to widening Howard Road and changing it to a two-way street due to safety and traffic concerns
- Opposition to realigning Suitland Parkway due to potential impacts on planned developments;
- Concern about construction impacts, such as noise, pollution, and traffic on residents of the area
- Concern about construction impacts on Anacostia River traffic;
- Concern about future regional traffic conditions resulting from the South Capitol Street project; the 11th Street Bridges project; and planned residential, commercial, and mixed-use developments;
- Concern that there should have been more community involvement with neighborhoods and ANCs during the South Capitol Street project development process;
- Concern that there is not enough coordination occurring between the many transportation and development projects in the region; and

- Concern that the South Capitol Street project would negatively affect planned developments in the area, such as Sheridan Terrace

2.12.4 Ongoing Coordination

Since the South Capitol Street Corridor FEIS was signed in March 2011, staff has continued to keep the general public and other important stakeholders informed about the project progress and preliminary design, financing and early property acquisition in a variety of methods. These methods include:

- Community organization presentations and/or attendance at community meetings;
- One-on-one stakeholder meetings and communication;
- Press releases issued through Mayor Vincent C. Gray's Office and the District Department of Transportation, as well as media outreach and coverage of project overview presentations and tours of the Frederick Douglass Memorial Bridge;
- AWI Website postings of design renderings, animation and other information on a regularly updated project page, as well as periodic informational blog postings, since launch of the website as www.anacostiawaterfront.org in May 2012;
- Continued development of a project stakeholder email contact list to which periodic information and updates about the project are sent and inclusion of information and updates in monthly AWI Newsletter; and
- Project Sponsored Meetings Informational Update Meeting in July 2013, Pre-Statement of Qualifications Submission Meeting in June 2013 and Industry Day Meeting in January 2013 after which all materials were further publicly disseminated via posting on the AWI website.

Community Organization Presentations/Attendance

- Nine Ward 8 Transportation Task Force Meeting presentations (April, May, September and December 2013; February, June, July and October 2012, November 2011) with attendance to answer questions at two other meetings (May and September 2012)
- One joint Bicycle Advisory Council/Pedestrian Advisory Council Meeting (February 2013)
- One Capitol Riverfront BID Board of Directors Meeting (January 2013)
- One Barry Farms Resident Advisory Council Meeting (May 2012)
- Two Greening Anacostia Metro Station Charette Meetings (February 2012)

One-on-One Stakeholder Meetings

- Two Capitol Riverfront BID meetings,
- One WABA meeting (January 2012),
- One Earth Conservation Corps meeting (May 2012),
- Two Chairman of the Ward 8 Transportation Task Force meetings,
- DDOE
 - November 5, 2012
- FHWA
 - November 11, 2012
 - December 18, 2012
 - February 27, 2013
- Inter-agency (DDOE, COE, NPS, Navy, USCG)
 - October 9, 2012
 - December 6, 2012
 - February 7, 2013
- US Coast Guard
 - March 3, 2013
 - June 5, 2013
 - August 8, 2013
- Corps of Engineers only
 - August 27, 2013
- US Navy
 - February 7, 2012
 - April 3, 2012
 - July 2, 2012
 - August 14, 2012
 - October 9, 2012
 - December 4, 2012
 - February 5, 2013
 - March 26, 2013
 - May 23, 2013
- Washington Nationals
 - June 19, 2013
- Office of Planning
 - January 29, 2013
- Capitol Riverfront BID
 - January 30, 2013
- Bicycle and Pedestrian Advisory Councils
 - February 5, 2013
- Public Meeting
 - July 30, 2013
- National Park Service
 - December 18, 2012
 - January 10, 2013
 - June 6, 2013
- Architect of the Capitol
 - January 15, 2013

- Ward 8
 - May 24, 2012
 - July 10, 2012
 - October 9, 2012
 - May 14, 2013
- Congresswoman Norton
 - January 22, 2013
- Utility Meetings – On-going with all impacted companies
- *Press Releases/Media Outreach/Coverage*
 - Design-Build Teams Submit Statement of Qualifications for Construction of South Capitol Street Corridor Segments 1 and 2 press release (August 2013)
 - Reminder - South Capitol Street Corridor Public Informational Update Meeting press release (July 2013)
 - South Capitol Street Corridor Public Informational Update Meeting press release (July 2013)
 - DDOE Issues Request for Qualifications for South Capitol Street Corridor Segments 1 and 2 press release (June 2013)
 - Megaproject Update: DDOE Preparing Request for Qualifications (RFQ) for an Integrated Premium Transit System and South Capitol Street Corridor Projects - (June 2013)
 - Mayor Gray's FY 2014 Budget Includes Funding for New Douglass Bridge press release (March 2013)
 - Regional Media project update, interviews and tours of Frederick Douglass Memorial Bridge (January 2013) which reached millions of people both in the District and larger metropolitan area by being covered in five print and online articles, 14 television stories and two stories that aired repeatedly on radio.
 - Interview by Greater, Greater Washington (January 2013)

2.13 Schedule for Construction

Construction Industry outreach	January 14, 2013
Issue Design Build RFQ	1st Quarter 2013
Complete Preliminary Engineering	2nd Quarter 2013
Issue Design Build RFP	3rd Quarter 2013
Douglass Bridge Contractor Selected	2nd Quarter 2014
Begin Construction	4th Quarter 2014
Complete Segments 1 and 2	Mid 2018

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3.0 Background

3.1 Project Location

The existing bridge carries South Capitol Street over the Anacostia River. South Capitol Street was a primary corridor in Major Pierre L'Enfant's 1791 Plan of the City of Washington and was envisioned as one of the symbolic gateways to the city and its Monumental Core. As envisioned in L'Enfant's Plan, South, East, and North Capitol streets represent the cardinal directions extending from the U.S. Capitol. As such, these streets are critically important within the hierarchy of Washington's streets and are considered to be "prominent gateways" to the Monumental Core.

The bridge is located at the southeast quadrant of the District of Columbia and immediately northeast of the confluence of the Potomac and Anacostia Rivers. The Washington Navy Yard is directly upstream from the existing bridge. The ballpark for the Washington Nationals Major League Baseball team is directly north of the bridge. Southeast of the bridge is the U.S. Naval Support Facility Anacostia, and northeast of the bridge is Poplar Point.



Image 3-1 Existing Bridge Location

3.2 Regional Context

The South Capitol Street Corridor Segments 1 and 2 project site straddles the Anacostia River, within the city limits of Washington, D.C.

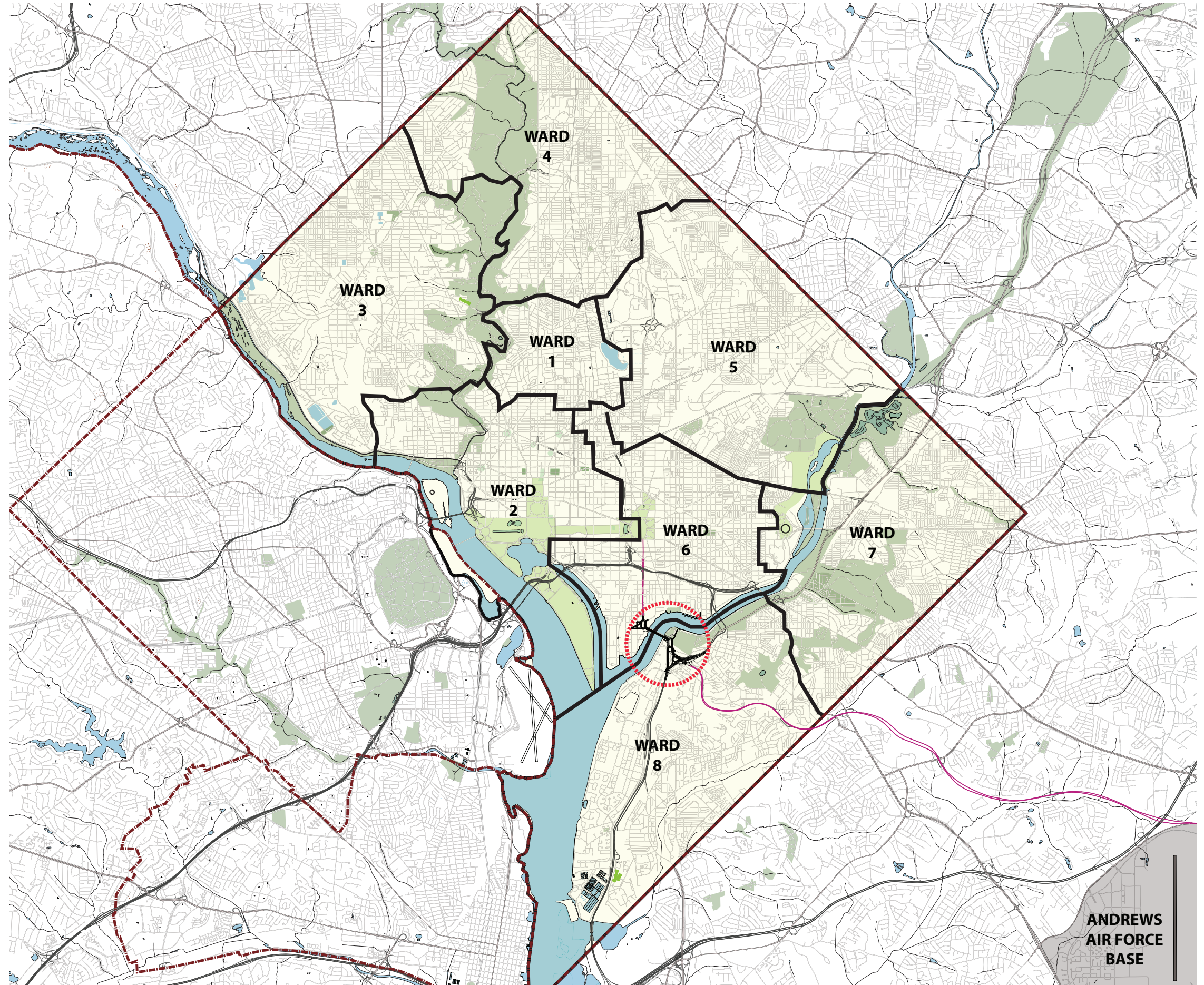
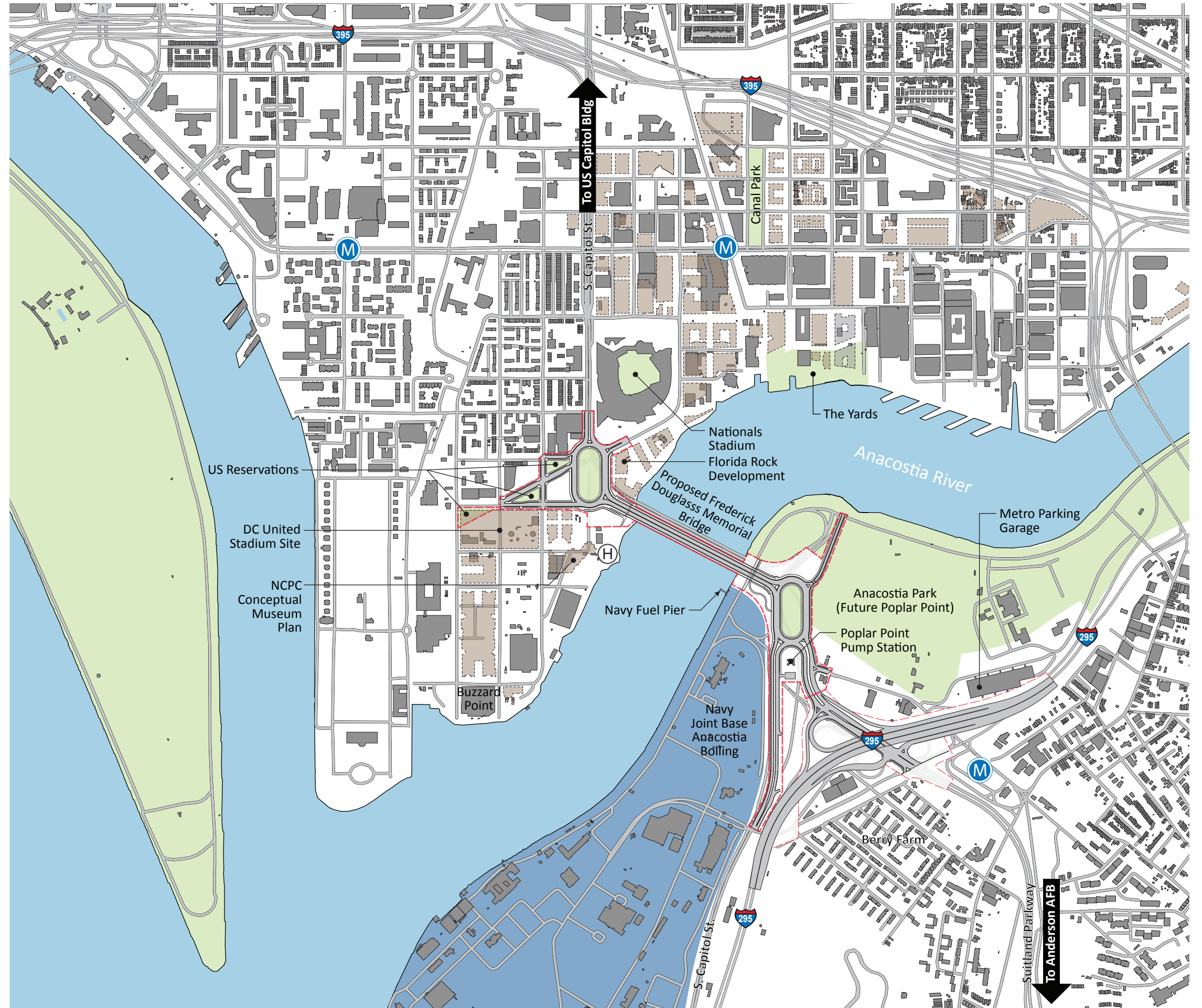
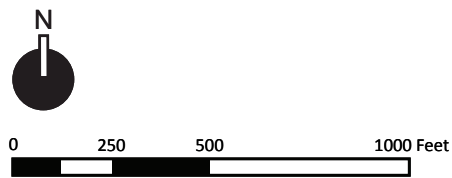


Figure 3-1 Vicinity Location Plan

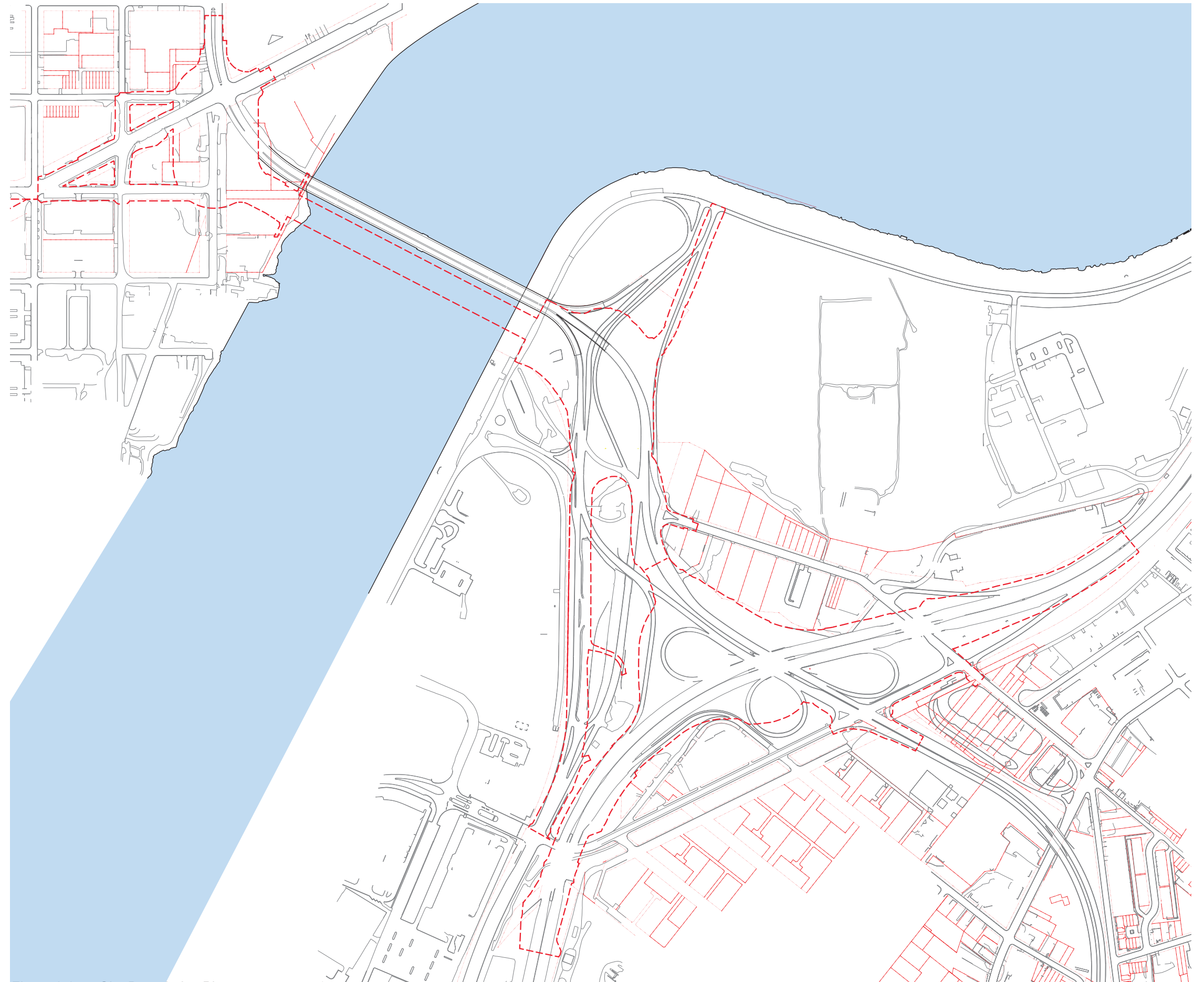
3.3 Local Context

LEGEND

-  Existing Building
-  Proposed Building
-  Parks
-  Navy Property
-  Limit of Work
-  Heliport
-  Metro Station
-  Interstate Highway



3.4 Site Boundaries



LEGEND

 Limit of Work



0 500 1000 2000 Feet

Figure 3-2 Site Boundaries Plan

3.5 Existing Conditions

3.5.1 Existing Historic Resources to Remain

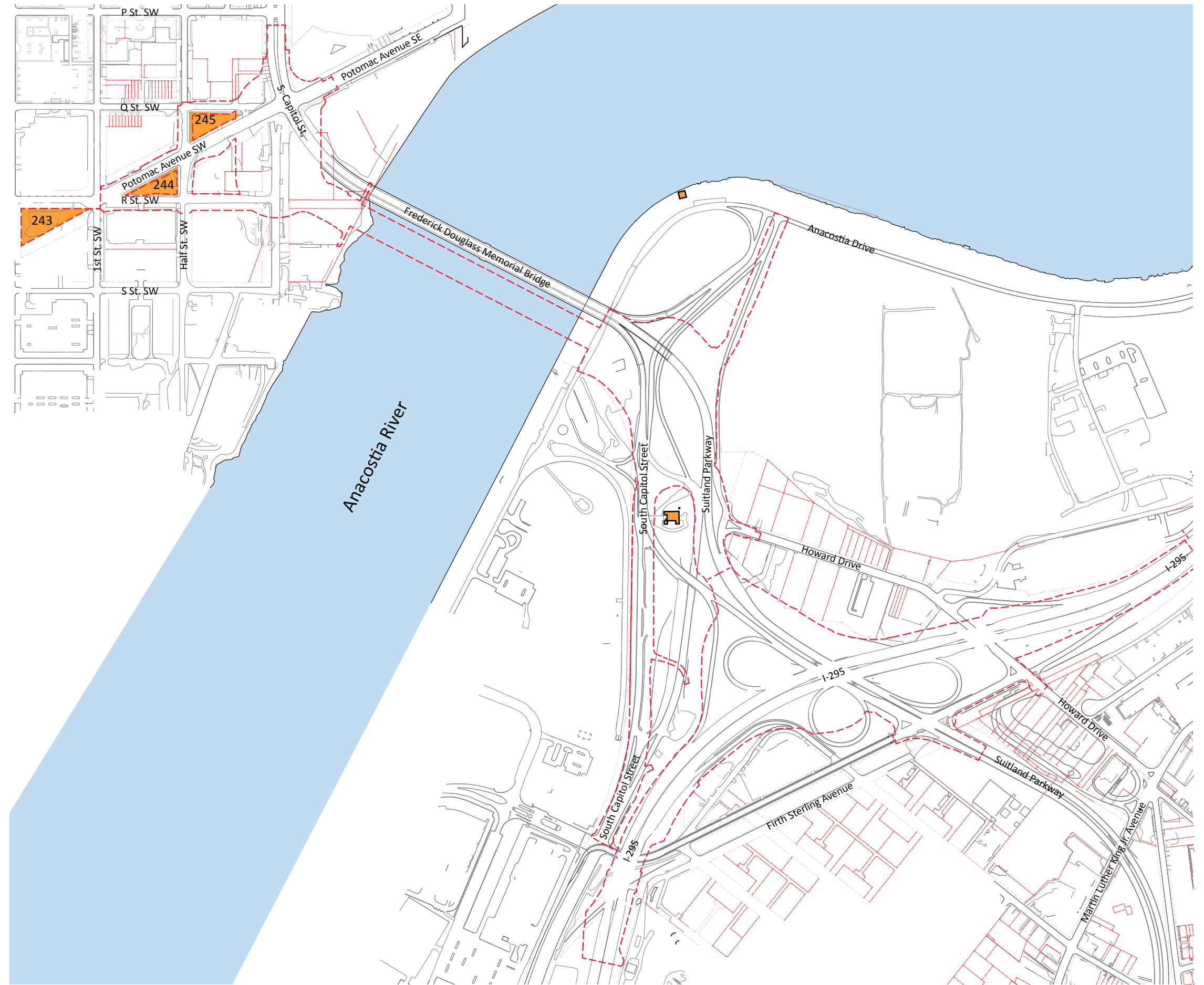




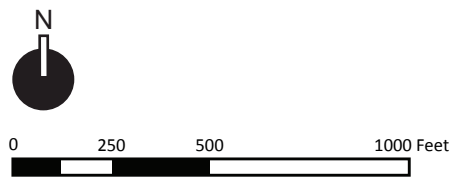


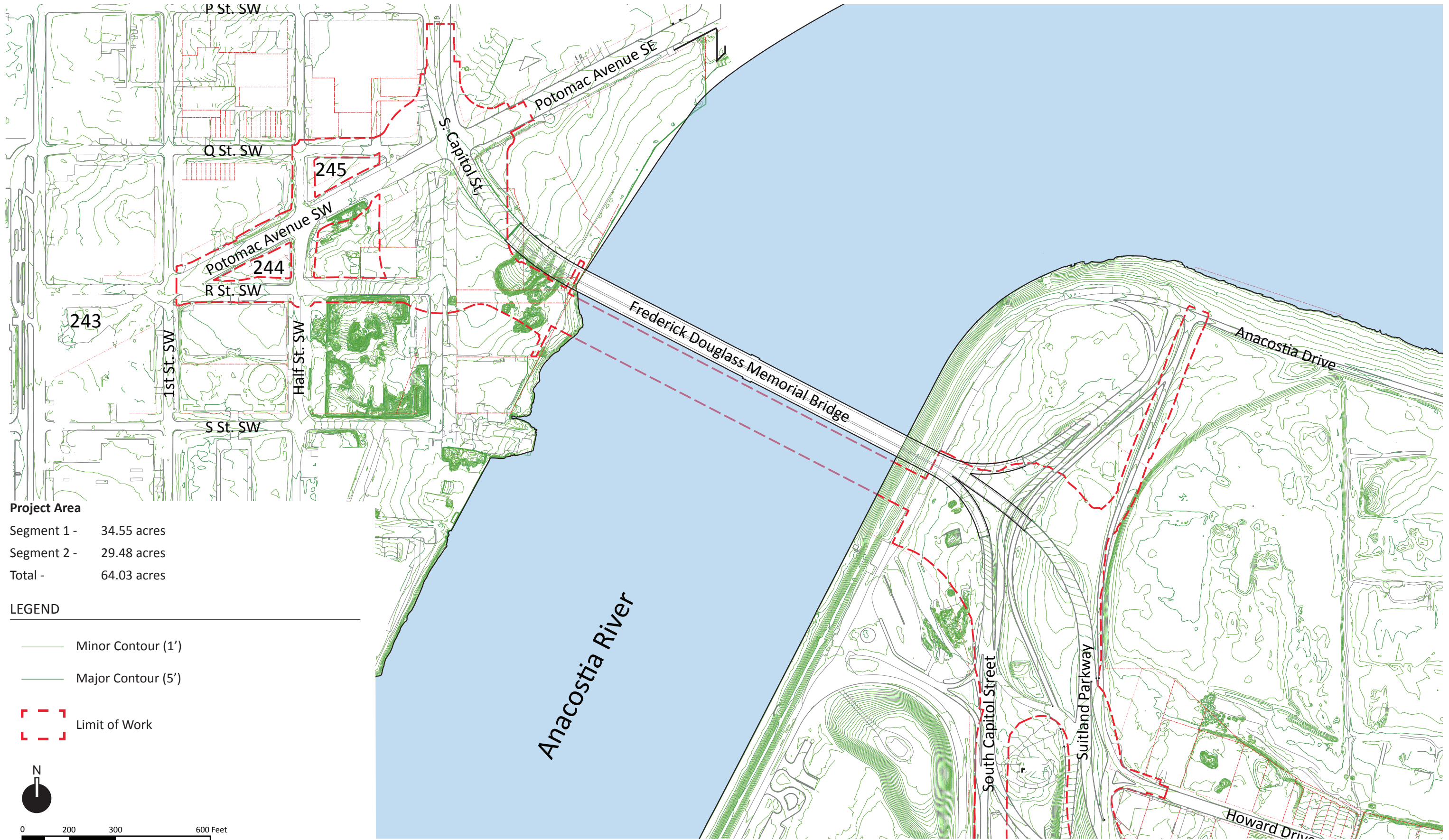
Figure 3-3 Existing Structures to Remain - Plan

LEGEND

-  Anacostia Pump House
-  Poplar Point Pump House
-  US Reservations
-  Limit of Work



3.5.2 Existing Topography



Project Area

Segment 1 -	34.55 acres
Segment 2 -	29.48 acres
Total -	64.03 acres

- LEGEND**
- Minor Contour (1')
 - Major Contour (5')
 - - - Limit of Work

Figure 3-4 Existing Topography Plan - Segment 1

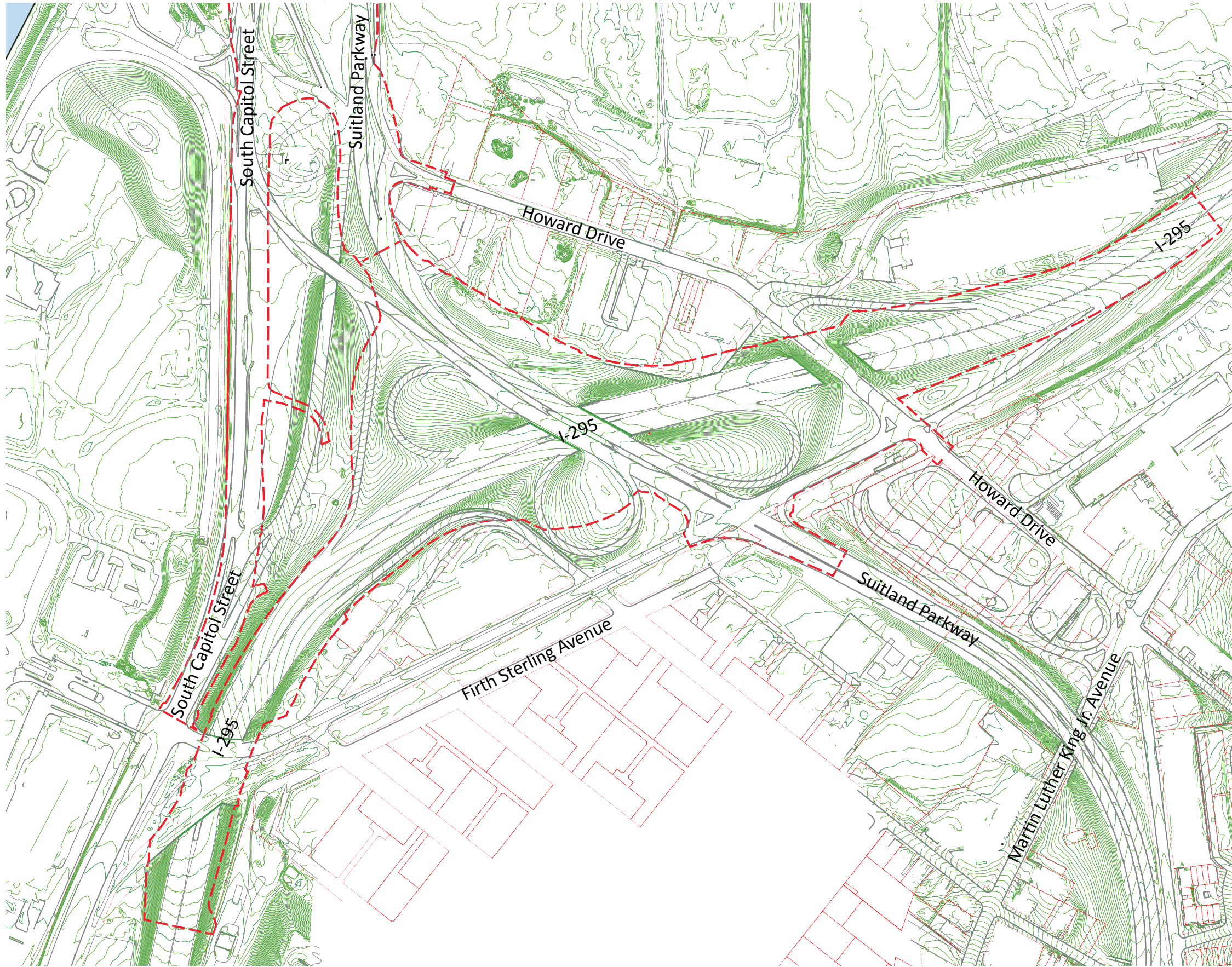


Figure 3-5 Existing Topography Plan - Segment 2

3.5.3 Site Photos - South Capitol Street



Image 3-2 View of the Capitol Building from South Capitol Street



Image 3-3 South Capitol Street - Potomac Avenue intersection looking south



Image 3-4 South Capitol Street - R Street intersection looking west



Image 3-5 South Capitol Street - Potomac Avenue intersection looking east



Site Photos - South Capitol Street



Image 3-6 South Capitol Street streetscape east sidewalk

Image 3-7 South Capitol Street streetscape west sidewalk



Image 3-8 Existing garage and UHAUL building

