

Replacement of North Drive Bridge



Environmental Assessment

Prepared for the
National Institutes of Health by:



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ENVIRONMENTAL ASSESSMENT

NIH NORTH DRIVE BRIDGE

NATIONAL INSTITUTES OF HEALTH

MONTGOMERY COUNTY

BETHESDA, MARYLAND

March 2014

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ABSTRACT

The National Institutes of Health (NIH) is proposing to replace the North Drive Bridge on the main NIH campus in Bethesda, Maryland. The project is needed because the existing bridge has broken beams and cracked decking, resulting in a useful life of less than five years if unimproved.

Two alternatives were considered in detail in the Environmental Assessment. The Proposed Action is to demolish the existing North Drive Bridge and replace it with a new two-lane bridge in the same location using Accelerated Bridge Construction to minimize traffic disruption. The No Action Alternative has the existing bridge remaining in place and in use until it has to be permanently closed for safety reasons.

The Preferred Alternative of NIH is the Proposed Action alternative. This Environmental Assessment will be submitted to the Maryland State Clearinghouse and will be available for public review. Comments should be sent to Valerie Nottingham at the above address.

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ES. Executive Summary

ES 1 Background

This Environmental Assessment (EA) has been prepared in compliance with the National Environmental Policy Act of 1969 (NEPA), as amended (Title 42, US Code [USC], 4321-4347), and regulations of the Council on Environmental Quality (CEQ) (40 Code of Federal Regulations [CFR] 1500-1508) as well as the US Department of Health and Human Services (HHS) General Administrative Manual (GAM) Part 30. This EA has been prepared for the National Institutes of Health (NIH).

This EA provides NIH with the information needed to assess the proposed project's potential effects on the environment. This EA contains a review of natural, cultural, and socioeconomic/community resources, and incorporates or references relevant environmental documentation.

The NIH campus in Bethesda, Maryland is located in Montgomery County inside the Capital Beltway along the northern boundary of the Bethesda Central Business District. This is the main NIH campus and it contains numerous laboratories for scientific investigation.

ES 2 Proposed Action

The NIH proposes to replace the existing North Drive Bridge over an unnamed tributary to Rock Creek.

ES 3 Purpose and Need

The purpose of the project is to improve safety and to restore full traffic operations at the North Drive entrance to the NIH campus. Based on structural analysis and visual observation of the bridge, concrete and timber support beams are permanently damaged and broken due to loading beyond the maximum allowable limits. The project is needed because, if unimproved, the existing North Drive Bridge would have a useful life of less than five years.

ES 4 Principle Conclusions

Implementation of the Preferred Alternative by demolishing the existing North Drive Bridge and replacing it with a new two-lane bridge in the same location using Accelerated Bridge Construction would likely result in minor negative environmental impacts. However, these impacts, direct or cumulative, are not anticipated to be significant. There would be no adverse effect to historic properties. Minor instream work would require the submission of a Joint Federal/State Application for the Alteration of Any Floodplain, Waterway, Tidal, or Nontidal Wetland in Maryland to the Maryland Department of the Environment.

Implementation of the No Action Alternative would result in the permanent closure of the existing North Drive Bridge, and thus the closure of the North Drive campus entrance.

1. Introduction

1.1 Background

The NIH is the one of the top medical research centers in the world and the center of Federal medical research in the US. The goal of NIH is to conduct research which leads to greater knowledge about the detection, diagnosis, and treatment of diseases and disabilities. In order to achieve that goal, more than eighty percent of the total NIH budget is expended in the form of peer-reviewed, competitively-awarded research grants, cooperative agreements, and contracts to nearly 50,000 principal investigators at more than 3,000 institutions across the country including universities, medical schools, and hospitals. In addition, some 2,000 research projects are conducted in the NIH intramural laboratories and at the NIH Clinical Center. Research is conducted at both the basic and clinical levels, encompassing studies related to the prevention, diagnosis, treatment, and cure of the many diseases that afflict the men, women, and children of the world. In addition, the basic research supported by NIH provides the foundation for the nation's pharmaceutical and biotechnology industries.

The Bethesda Campus of NIH is inside the Capital Beltway along the northern boundary of the Bethesda Central Business District located in Montgomery County, Maryland. The NIH has planned campus growth to be compatible with the surrounding area, especially with residential and commercial areas in the Bethesda-Chevy Chase community. The more than 90 buildings serving NIH's Institutes and Centers are all located on the 308-acre Campus. The 2013 Draft Master Plan for the Bethesda Campus states that the 2011 campus population was 20,594. The draft Master Plan proposes an approximate maximum campus employee population of 24,000 by 2033. To support the potential growth in campus employees, the campus gross built area would increase during the 20 year Master Plan period from approximately 11.9 million gross square feet (gsf) to 14.25 million gsf. The Draft Environmental Impact Statement for the Master Plan is being prepared and is anticipated to soon be available to the public.

1.2 Study Area Location

MD 355 is a 37-mile north-south road which has interchanges with both the Capital Beltway (I-495) and I-270. The portion of MD 355 which runs along the eastern border of the NIH Bethesda campus is named Rockville Pike. This roadway is a major artery which links a mixture of institutional, commercial, and residential uses. There are six vehicle entrances into the NIH campus off of Rockville Pike. One entrance is reserved for visitors, a second entrance is reserved for commercial vehicles that must be inspected before entering the campus, and the other four entrances (North Drive, Wilson Drive, South Drive, and Center Drive) are for NIH employees. An adjacent ongoing Maryland State Highway (SHA) project will reconfigure and widen shared use paths and reconfigure the entrance at the Rockville Pike and North Drive intersection.

The North Drive entrance is located in the northeastern corner of the NIH campus in Bethesda, Maryland (**Figure 1**). The North Drive Bridge traverses an unnamed stream 150 feet west of MD 355. North Drive is a one-way two-lane road that leads from southbound Rockville Pike west to the campus. The bridge is currently open only to entering employee traffic and only between 6:00 AM and 10:00 AM daily. The bridge is currently restricted to one lane.

The existing superstructure is a 25-foot 10-inch wide deck with 13-inch wide stone railings on each side for a total bridge width of 28 feet x 38 feet 7 inches long. The bridge has a three-span simply supported reinforced concrete deck on timber stringers and cantilevered girders, with three equal spans of 12 feet each. The bridge is supported by 15-inch diameter timber piers set atop a strip concrete footing and bent spill-through abutments at each end.

Pending agency review, the study area does not contain any wetland systems. One waters of the US (WUS) was identified during the field investigation. WUS WL001 is a nontidal, perennial stream that flows north, paralleling Rockville Pike, under the North Drive entrance bridge, and continues to flow northeast where it exits the property at a culvert crossing Rockville Pike. Approximately 227 linear feet (LF) of this stream is within the study area. Impacts to wetlands or waterways within the proposed project area will require a Joint Federal/State Application for the Alteration of Any Floodplain, Waterway, Tidal, or Nontidal Wetland in Maryland. There is no FEMA-designated 100-year floodplain within the study area.”

1.3 Scope of EA

This EA provides NIH with the information needed to assess the proposed project’s effects on the environment and to make an informed decision. It contains a review of natural, cultural, and socioeconomic/community resources, and all appropriate environmental documentation.

1.4 Relationship to Other Activities

The North Drive Bridge replacement would allow for better traffic movement within the NIH campus and for visitors coming to and from the campus via Rockville Pike. The Maryland State Highway Administration (SHA) has been working on a utility project at the intersection of Rockville Pike and Cedar Lane. The SHA project is scheduled to be completed at the end of spring 2013.

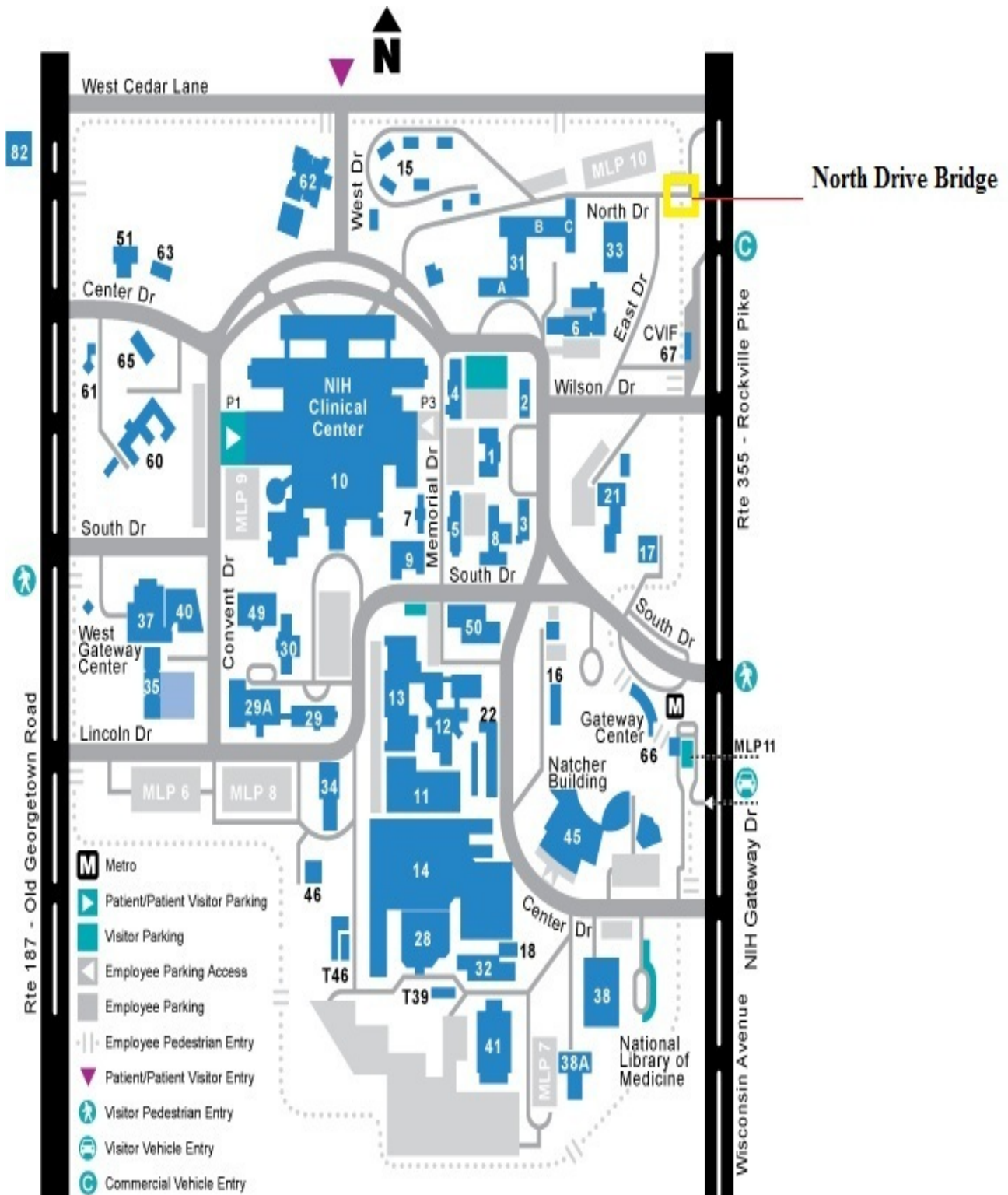


Figure 1: North Drive Bridge Study Area

2. Purpose and Need

2.1 Project Purpose

The purpose of the NIH North Drive Bridge Project is to improve the safety of the stream crossing and to restore traffic operations along North Drive. The bridge has not had major repairs since 1976 when the structure was rehabilitated with the replacement of the original 2-inch thick timber plank deck with an 8-inch thick reinforced concrete deck placed into corrugated steel decking form.

The NIH Police provided anecdotal average daily traffic counts of 250-300 vehicles using the bridge between 6:00 AM and 10:00 AM. Currently there is no load limitation sign posted at the bridge site. However, the southern lane of the two-lane westbound bridge has been closed to traffic because of the condition of the bridge. (See **Figure 2.**) The northern lane of the bridge remains open to traffic.



Figure 2: North Drive Bridge partial closure

2.2 Project Need

The *Structural Condition Assessment for North Drive Bridge*, (Maiden & Associates, 2012) found that the North Drive Bridge has visual evidence of broken beams and cracked decking. The damage to the existing bridge includes:

- 1) The cantilevered portion of the end timber girder (beam) has been permanently damaged because of loading beyond the maximum allowable compressive load capacity of the material. The crushing pattern effect of the 8-inch x 12-inch girder directly above the column indicated a continually type Live Load usually caused by a heavy vehicle.
- 2) All timber columns show no signs of settlement, deformation, or lateral movement. The timber columns are set into concrete pile cap foundation/footing. The concrete has shown signs of spalling and deterioration due to erosion from the stream. (See **Figure 3.**)



Figure 3: North Drive Bridge Substructure

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- 3) The concrete spandrel beam under the stone railing on the south side of the bridge has rotated approximately 8 degrees clockwise. The rotation of the spandrel beam is caused directly by the rotation of the timber girder upon which it partially rests; and due to insufficient anchors/footings at each end of the spandrel beam.
 - 4) The existing 6-inch x 14-inch timber stringers (joists) atop the girders (beams) underneath the concrete deck are in good condition.
 - 5) The galvanized corrugated steel has begun to corrode. The 7-inch embedded concrete topping has not been compromised due to the corrosion. Concrete tests reveal that the existing concrete deck is of sufficient strength to cars (only) (up to about 4,000 pounds) but the underlying timber structural frame has been damaged and has less than the required rating for vehicular use, except at the center portion of the bridge.
 - 6) The existing piers supporting the bridge are un-pinned and un-anchored at the base. It shall be noted that this un-pinned and un-anchored pier base condition causes the bridge to have a significantly reduced load bearing capacity.

Based on structural analysis and visual observation of the bridge, concrete and timber support beams are permanently damaged and broken due to loading beyond the maximum allowable limits. The bridge cannot currently support an H-15 loading since two pier bents have failed due to insufficient load carrying capacities. A pier bent is a row of placed piers with a pier cap to hold them in their correct positions. (An H-15 bridge is based on a hypothetical vehicle with one 6,000 pound axle and one 24,000 pound axle.) This project would replace the bridge with a new structure with two-vehicle lanes to support vehicle loading of 40-T or HS-25 truck loading. A new structure would allow for North Drive to be fully re-opened and allow for the passage of traffic between MD 355 and the NIH campus. Additionally, the life expectancy of a new bridge would be approximately 50 years.

Figure 4: Bridge Loadings

An **H-15** loading is represented by a two-axle single unit truck weighing 30,000 pounds (15 tons) with 6,000 pounds on its steering axle and 24,000 pounds on its drive axle. (Source: *Comprehensive Truck Size Weight Study, Vol. 3, US Department of Transportation, 2000*).

An **HS-25** loading is represented by a three-axle semitrailer combination weighing 90,000 pounds with 10,000 pounds on its steering axle, and 40,000 pounds on its drive axle and 40,000 pounds on the semitrailer axle. The "25" in HS-25 stands for 25 tons (5 tons on the steering axle and 20 tons on the drive axle). The "S" stands for semitrailer combination which adds the additional 20 tons for the third axle to give a total of 45 tons or 90,000 pounds.

3. Alternatives

3.1 Proposed Action

The Preferred Alternative of NIH is to demolish the existing North Drive Bridge and replace it with a new two-lane bridge in the same location using Accelerated Bridge Construction (ABC) to minimize traffic disruption. The Federal Highway Administration defines ABC as bridge construction that uses innovative planning, design, materials, and construction methods in a safe and cost-effective manner to reduce the onsite construction time that occurs when building new bridges or replacing and rehabilitating existing bridges. ABC can reduce project delivery time, thereby also reducing traffic impact and onsite construction time.

3.1.1 Site and Construction

The proposed bridge span will be 12 feet longer than the existing bridge to span over the existing bridge foundations. The existing bridge elements will be removed to just below the surface limiting disturbance of the stream bed. The construction impact of the proposed bridge will be controlled to minimize disturbance of the existing stream bank. Some reconfiguration of the bank under the bridge will be required to construct the bridge and to provide access for inspection of the bridge abutments in the future. The disturbed area of the stream bank will be replanted with native vegetation and the stream bed reinforced and armored with natural stones/boulders similar to the work completed in the stream naturalization project just south of the bridge.

The four utility conduits that are currently supported off the north side of the bridge will be detached and maintained during the bridge replacement construction. On the proposed bridge, the conduits will be routed through the proposed parapets as the existing concrete duct banks will likely be exposed in the re-contouring of the bank at the edge of the bridge. This routing will protect the conduits from damage as well as aesthetically conceal them from view.

3.1.2 Description and Operations

The replacement structure is proposed to be a 50'-0" long by 30'-0" wide single span bridge. The design loading is HS-25, which is equivalent to a 45-ton vehicle. To accommodate ABC, the bridge will be supported by two precast concrete abutment pile caps founded on a deep foundation. The bridge superstructure will be precast prestressed adjacent slabs with a cast-in-place concrete overlay. The bridge typical section will consist of a 28-foot clear roadway width (two 12-foot lanes and two 2-foot offsets) and two 2-foot wide parapets.

The horizontal and vertical alignment of the new roadway will have minimal adjustments from existing conditions. Drainage on the bridge would be investigated during final design.

3.1.3 Safety and Security

The use of the new one-way North Drive Bridge will primarily be controlled by the existing security checkpoint at the North Drive entrance to the NIH campus. This entrance is planned to be open only from 6 AM to 10 AM on weekdays. Access to the bridge is also controlled by

retractable bollards installed in the road surface just east of the bridge. The bollards are erected when the North Drive entrance is closed. Additionally a normally- locked gate in the perimeter fence restricts direct access at Rockville Pike.

Pedestrians will not be permitted on the new bridge. Although AASHTO standards dictate 34-inch high parapets on the bridge, the new parapets are designed at a height of 42 inches for the protection of bicyclists.

3.1.4 Transportation

The proposed North Drive Bridge will allow two-lanes of inbound vehicle traffic entering the campus. It will safely accommodate vehicles up to 45 tons. Pedestrians may access the NIH campus via a pedestrian bridge 100 feet south of the North Drive Bridge.

3.1.5 Utilities and Waste Management

The proposed bridge will continue the utility capability provided by the existing bridge. All utility conduits on the existing bridge will be transferred to the new bridge. Waste management does not occur in the study area and would not be affected by the new bridge. NIH policy is to attempt to recycle up to 100% of construction debris when feasible. NIH would likely include this requirement in the specifications for the construction contractor.

3.2 No-Action Alternative

Under the No-Action Alternative, the replacement of the North Drive Bridge would not proceed. The existing bridge would remain in place and be in use until it had to be permanently closed for safety reasons. As only one side of the bridge is currently operational, traffic flow would remain limited. The bridge is nearing the end of its service life, therefore, this option would likely result in the closing of the bridge in the near future.

3.3 Alternatives Considered and Dismissed

The alternatives listed herein are based on options described in the January 2012 Condition Assessment.

3.3.1 Alternative 1: Repair Existing Bridge

Alternative 1 includes repairing the existing bridge and regulating the bridge to one-way traffic. Restricting the vehicular traffic to one lane in the center portion of the bridge would eliminate live loading over the cantilevered portion of the bridge and allow for pedestrian walkway lanes on both the north and south sides of the bridge. The broken girder would be jacked to a level position and steel flitch plates installed at both sides of all cantilevered girders, overlapping and extending half the width of the bridge. A new clear span steel beam would be installed under the cantilevered end of the girders. The new steel beam shall rest on a new cast-in-place concrete abutment footing at each end. The existing concrete deck would remain. The existing stone railings and temporary concrete “Jersey Barriers” would be removed and a galvanized steel railing would be installed at both sides of the vehicular traffic lane.

Alternative 1 was dismissed for several reasons. The maximum load capacity for this bridge option is H-10. Further, the estimated remaining life of the bridge is approximately five years with this alternative. The existing piers are not anchored to the concrete footings and the condition of the concrete footings has revealed signs of spalling. The main structural elements (piers and footing) are nearing the end of their service life. The integrity of the structural components is weakened and they will fail to behave as a unified system. Thus, complete bridge replacement would be required within the estimated remaining life of five to eight years or in the event of a major earthquake or flood.

3.3.2 Alternative 2: Replacement Onsite & Single Lane Bridge

Construction would be in two phases to allow continued vehicular traffic use of the existing bridge. Phase I would construct a new single lane bridge adjacent and to the north of the existing bridge while the existing bridge remains in place and in use. Phase II would open the new single-lane bridge for vehicular use. The existing bridge would be closed, razed, and replaced onsite with a new slab precast / prestressed concrete bridge on clear span steel girders with reinforced concrete slab roadway topping. The maximum load capacity for this bridge option is HS-25/HS-27. The new bridge would include two lanes for two-way traffic. The single lane bridge constructed in Phase I would be used for a pedestrian lane and emergency third lane.

Alternative 2 was dropped primarily because it would result in greater environmental impacts than the Preferred Alternative. Additional clearing of trees and vegetation would be required. Alternative 2 would involve greater linear feet of stream impact and for a longer period of time. The second bridge would also create increased stream shading and increased runoff from additional impervious surface. While traffic disruption may be minimized, and useful life would be 50 years, Alternative 2 would also produce increases in environmental impact, construction time, and cost.

3.3.3 Alternative 3: Construct a New Bridge South of the Existing Bridge

Alternative 3 would involve constructing a new bridge 100 feet south of the existing bridge and razing the existing bridge. The new structure would be a precast/prestressed concrete slab bridge on clear span steel girder structural frame, spaced at six feet off center. Cast-in-place concrete abutments would be situated at each end. The new bridge would be a two-way, three-lane bridge with a 5-foot wide pedestrian walkway on the south side of the bridge. To protect pedestrians, 54-inch high galvanized steel railings would be installed on both sides of the bridge and between the pedestrian walkway and vehicle lanes.

Although useful life would be 50 years, Alternative 3 was dropped from consideration because of the increased environmental impact resulting from clearing and construction associated with relocating the roads approaching the bridge. The increased impervious surface of the new roads would require additional stormwater management and erosion and sediment control.

Additionally the relocated roads would require a new traffic pattern on the NIH campus and possibly new access to MD 355. The resulting cost of this alternative would be greater than the Preferred Alternative.

4. Affected Environment

4.1 Topography, Geology, and Soils

4.1.1 Topography

The topography of the area consists of rolling hills with steeper slopes commonly found along streams. Slopes are generally 15 percent or less. The average elevation of the area ranges from 200 to 400 feet above sea level. According to the *Kensington, Maryland 7.5' Topographic Quadrangle* (United States Geological Survey, 1979) and other sources, the topography within the study area is generally flat, sloping towards the north. Elevations range from approximately 230 feet above mean sea level (MSL) to 240 feet above MSL. The surrounding area is flat and developed, with Rockville Pike to the east, and NIH facilities to the west.

4.1.2 Geology

Based on the *Physiographic Provinces and their Subdivisions in Maryland* (2011), the North Drive Bridge study area is located in the eastern Piedmont Plateau Physiographic Province. The Piedmont Plateau Province is composed of hard, crystalline igneous and metamorphic rocks. The bedrock in the study area consists of schist, gneiss, gabbro, and other highly metamorphosed sedimentary and igneous rocks of probable volcanic origin. The *Geologic Map of Maryland* (1968) indicated that the bedrock in the project area belongs to the Lower Pelitic Schist formation. This formation consists mainly of coarse grained, biotite-oligoclase-muscovite-quartz schist, and fine to medium grained semipelitic schist. A variety of mineral resources are also contained in the Piedmont Plateau.

4.1.3 Soils

According to the *Soil Survey of Montgomery County, Maryland* (United States Department of Agriculture-Soil Conservation Service [USDA-SCS], 1995) and recently available digital Natural Resources Conservation Service (NRCS) Soil Survey Geographic Database (SSURGO) soils data for the county (NRCS Web Soil Survey, 2013), the predominant soil association found within the vicinity of the study area is the Urban Land-Wheaton-Glenelg Association. Soils in this association are urban land and nearly level to strongly sloping, well drained, very deep soils that are loamy throughout; on uplands. Within this association, one distinct soil unit is present within the study area, Baile silt loam, 0-3 percent slopes.

The mapped soil unit is classified hydric based upon its listing on the *National Hydric Soils List by State* (USDA-NRCS, 2012) and the State and county lists in the web soil survey (NRCS Web Soil Survey, 2013). Hydric soils are defined as those soils that are saturated, flooded, or ponded long enough during the growing season to develop anaerobic conditions in the upper part of the soil profile. The study area is dominated by the hydric Baile silt loam on 0 to 3 percent slopes.

4.2 Groundwater/Surface Water

An unnamed tributary to Rock Creek flows through the study area. Rock Creek and its tributaries are classified as Use I streams. The Maryland Department of Environment defines Use I streams as being for Water Contact Recreation, and Protection of Nontidal Warmwater Aquatic Life. In-stream work may not be conducted during the period of **March 1 through June 15**, inclusive during any year (COMAR 26.08.02.11). This provides protection of selected wildlife resources in times of the year during which certain species may be most sensitive to human activities such as construction. According to a review of Federal Emergency Management Agency (FEMA) Q3 flood data, no designated 100-year floodplains are present within the study area (*FEMA Panel No. 24031C0365D*).

4.3 Climate

The climate of Montgomery County is described as humid continental, characterized by frequent daily and seasonal temperature changes, abundant precipitation, warm summers, and cold winters.

4.4 Surrounding Communities

The large NIH campus is adjacent to several communities. Maplewood, Arlyawn, Oakmont, Sonoma, East Bethesda, Locust Hill, Huntington Terrace, and Edgewood/Glenwood consist predominately of single family detached dwellings. Mid-rise apartments and condominiums comprise the Battery Lane Apartments. The area around NIH is heavily developed and has limited potential for residential development.

4.5 Human Health

The project site is essentially a two-lane driveway over a small stream. There are no permanent residences in the immediate vicinity and human activity is within vehicles travelling past the project site. Overall, Montgomery County has the lowest age-adjusted death rates for all causes within the State of Maryland (Maryland Department of Health and Mental Hygiene, Maryland Vital Statistics 2005).

4.6 Socioeconomics

4.6.1 Demographics

In 2010, Montgomery County has a total population of 971,600 persons, up 20,920 (2.2 percent) from 2008 and 98,259 (11.3 percent) from 2000 (Montgomery County Snapshot, Council Districts by the Numbers, July 2010, Montgomery County Planning Department). In 2010, Bethesda, a Census Designated Place, had a population of 60,858 (2010 U.S. Census).

4.6.2 Employment

With a 2011 population of 20,262 workers at the Bethesda campus, NIH is Montgomery County's largest public sector employer.

4.7 Air Quality

The US Environmental Protection Agency (US EPA) established National Ambient Air Quality Standards (NAAQS) for the protection of public health and the environment (40CFR50). These standards establish limits for six specific criteria pollutants: nitrogen dioxide (NO₂), carbon monoxide (CO), 1-hour and 8-hour ozone (O₃), sulfur dioxide (SO₂), lead (Pb), inhalable particulate matter PM₁₀ (particles with aerodynamic diameter less than or equal to 10 microns), and PM_{2.5} (particles with an aerodynamic diameter less than or equal to 2.5 microns). The Washington region is a non-attainment area for PM_{2.5} and ground-level ozone. The NAAQS pollutant concentration most likely to be exceeded by vehicular traffic generated emissions is carbon monoxide (CO).

As NIH has a public health mission, it is attempting to reduce Greenhouse Gases (GHG) emissions from NIH activities. To this end NIH is developing an inventory of GHG emissions related to its operations.

4.8 Environmental Justice

Presidential Executive Order 12898, issued February 11, 1994, requires Federal agencies to ensure environmental justice as part of their overall mission by identifying and addressing disproportionately high and adverse human health or environmental effects of activities on minority and low-income populations.

Adverse health effects may include bodily impairment, infirmity, illness, or death. Adverse environmental effects include socioeconomic effects when those impacts are interrelated to impacts on the natural or physical environment.

The NIH has applied a “more than 25 percent” threshold of minority populations and low-income populations for analyzing potential Environmental Justice effects. This threshold is allowable under the Council of Environmental Quality (CEQ) guidance on Environmental Justice.

Disproportionately high and adverse human health effects are identified by assessing three factors:

- Whether the adverse health effects, which may be measured in risks or rates, are significant or above generally accepted norms. Adverse health effects may include bodily impairment, infirmity, illness, or death.
- Whether health effects occur in a minority population or low-income population affected by cumulative or multiple adverse exposures from environmental hazards.
- Whether the risk or rate of exposure to a minority population or low-income population to an environmental hazard significantly and appreciably exceeds or is likely to exceed the risk or rate to the general population or other appropriate comparison group.

Table 1, below, shows that the racial composition of Montgomery County (Census 2000) is over 57.5 percent White, 17.2 percent Black, and 13.9 percent Asian.

Table 1: Racial Composition - Montgomery County

Race	Number	%
White Persons	558, 358	57.5
Black Persons	167, 315	17.2
American Indian and Alaska Native Persons	3,639	0.4
Asian Persons	135,451	13.9
Native Hawaiian or Other Pacific Islander	522	0.1
Other	80,532	8.3

4.9 Waste Management

Although no waste management operations are presently occurring in the immediate vicinity of the North Drive Bridge, NIH policy is to attempt to recycle up to 100% of construction debris when feasible.

4.10 Noise

No noise sensitive receptors have been identified in the North Drive Bridge project area. The project area is adjacent to Rockville Pike which provides constant traffic noise.

4.11 Land Use

The North Drive Bridge is within the 250 foot-wide perimeter open space buffer of the NIH campus. Immediately west of the buffer, the campus land use includes roads, sidewalks, multi-level parking, and buildings. MD 355 (Rockville Pike) is east of the bridge, outside the NIH campus. The Naval Support Activity Bethesda, including the Walter Reed National Military Medical Center, is directly across Rockville Pike from the NIH Bethesda Campus.

4.12 Aesthetics

The existing bridge is within the 250 foot perimeter buffer of the NIH campus. A very narrow band of wooded riparian buffer protects the stream. The stone parapets of the existing bridge are compatible with the stone arch pedestrian bridge approximately 100 feet downstream. Utility conduits are hung from each stone parapet.

4.13 Ecological Resources

The study area is located inside the NIH campus, a developed area; however, the area is vegetated along the stream. Section 7 consultation is required under the Endangered Species Act of 1973 (ESA § 7) for any federal action that may take place within the habitat of any federally listed rare, threatened, or endangered (RTE) species. This consultation process ensures that actions taken by federal agencies will not jeopardize the existence of any listed species. Federal agencies are required to consult with the US Fish and Wildlife Service (USFWS) to ensure that they are not involved in any actions likely to jeopardize the continued existence of any listed species or critical habitat.

Correspondence was sent to USFWS and to the Maryland Department of Natural Resources (DNR) inquiring whether any state or federally-listed RTE species or anadromous fish have been found within the project study area. Only the DNR Environmental Review Program responded, indicating that no anadromous fish have been documented near the project area; however, the stream may support other resident fish species.

4.14 Transportation

Vehicular access to the NIH campus is via seven employee entrances, each with a security check point. In 2011, approximately 53% of the vehicle entering the campus in the morning peak period (8:00-9:00 AM) entered the campus from Rockville Pike. NIH visitors in automobiles must enter NIH through a secured visitor vehicle entrance on Rockville Pike and must park in a designated visitor's garage or have their vehicles inspected before entering campus.

The NIH has established a Transportation Management Plan (TMP) to reduce peak hour vehicular traffic by encouraging NIH employees who drive alone to ride-share, use public transportation, or use other alternative modes of transportation.

4.15 Historic Resources

Potentially significant architectural and archeological resources were both researched as part of the historic investigation associated with the proposed rehabilitation of the bridges. The Area of Potential Effects (APE) for this project was determined, in coordination with the Maryland Historical Trust (MHT), by considering potential physical and visual impacts to historic properties, including archeological sites and standing structures. The North Drive Bridge would be demolished and replaced in the same location with a new two-lane prestressed pre-cast concrete adjacent slab bridge. All proposed work will be completed within existing NIH right of way. Therefore the proposed APE is restricted to 50 feet on either side of the existing bridge and 50 feet of roadway at each approach. Proposed changes will not alter any viewsheds; therefore a larger APE is not warranted.

Architectural identification methods included consultation of the MHT web site and examination of Maryland Inventory of Historic Places (MIHP) forms. The North Drive Bridge is not

individually included in the MIHP or the National Register of Historic Places (NRHP). However, the NIH campus as a whole is on the MIHP. Additionally, the NIH Historic Core consisting of Buildings 1, 2, 3, 4, 5, and 6 are on the MIHP and are eligible for listing on the National Register (**Figure 1**). Building 6 is in excess of 500 feet away from the North Drive Bridge and is obstructed from its viewshed by Building 33.

Based on the GIS database of historic properties, there are no known archeological sites within the APE. The North Drive Bridge is located in an area that is primarily institutional.

4.16 Utilities and Services

Although utilities are concentrated under NIH campus roadways, buried utility lines crisscross the entire campus. The NIH Master Utility Plan recommends consolidating many utilities in utility corridors. As such, any construction on campus must take into consideration these existing utility lines. Underground utilities in the study area were coordinated with Miss Utility. Additionally a subsurface utility investigation was completed and utilities were marked and surveyed.

5. Environmental Consequences

5.1 Topography, Geology, and Soils

5.1.1 Preferred Alternative

The Preferred Alternative would have no impact on geology or soils. Topography underneath the bridge would be slightly lowered to provide access to the new bridge abutments.

5.1.2 No-Action Alternative

The No-Action Alternative would have no impact on topography, geology, or soils.

5.2 Groundwater/Surface Water

5.2.1 Preferred Alternative

As the unnamed tributary to Rock Creek is a Use I stream, construction of any build alternative would be avoided during the period of March 1 through June 15 during any year.

5.2.2 No-Action Alternative

The No-Action Alternative would have no impact on groundwater or surface water.

5.3 Climate

5.3.1 Preferred Alternative

The Preferred Alternative would have no impact on the climate of the region.

5.3.2 No-Action Alternative

The No-Action Alternative would have no impact on the climate of the region.

5.4 Surrounding Communities

5.4.1 Preferred Alternative

The Preferred Alternative would have no impact on the surrounding communities.

5.4.2 No-Action Alternative

The No-Action Alternative would have no impact on the surrounding communities.

5.5 Human Health

5.5.1 Preferred Alternative

The Preferred Alternative would have no impact on human health.

5.5.2 No-Action Alternative

The No-Action Alternative would have no impact on human health.

5.6 Socioeconomics

5.6.1 Preferred Alternative

The Preferred Alternative would have no impact on socioeconomics.

5.6.2 No-Action Alternative

The No-Action Alternative would have no impact on socioeconomics.

5.7 Air Quality

5.7.1 Preferred Alternative

The Preferred Alternative would have minimal impact on air quality. Minor greenhouse gas emissions would occur during the construction of the bridge. However the duration and subsequent impact would be minimized due to utilization of Accelerated Bridge Construction. Because the new bridge would have the same two-lane capacity as the existing bridge, vehicle emissions are likely to remain the same or drop as emission regulations increase over time. Therefore impacts to air quality from emissions from construction and operation of the North Drive Bridge would be minor.

The construction phase of the proposed project has the potential to minimally impact the local ambient air quality by generating fugitive dust through activities such as demolition and materials handling. To minimize such impact, an erosion and sediment control plan will be developed during design and followed during construction.

5.7.2 No-Action Alternative

The No-Action Alternative would have no impact on air quality.

5.8 Environmental Justice

5.8.1 Preferred Alternative

No disproportionately high or adverse effects on minority or low-income populations will occur as a result of the Preferred Alternative.

5.8.2 No-Action Alternative

The No-Action Alternative would have no impact on environmental justice communities.

5.9 Waste Management

5.9.1 Preferred Alternative

The Preferred Alternative would have no impact on waste management operations on the NIH campus. NIH policy is to attempt to recycle up to 100% of construction debris when feasible. NIH would likely include this requirement in the specifications for the construction contractor.

5.9.2 No-Action Alternative

The No-Action Alternative would have no impact on waste management.

5.10 Noise

5.10.1 Preferred Alternative

The Preferred Alternative would have no noise impact as no noise sensitive receptors have been identified in the North Drive Bridge project area. Traffic volume is not anticipated to increase as a result of the proposed structure. The types of vehicles utilizing the new bridge would be similar to those that used the existing bridge prior to the placement of restrictions.

Accelerated Bridge Construction requires off-site assembly of certain structure components, thereby minimizing construction noise and the duration of construction. Additionally, construction would be subject to any NIH specifications for the contractor and campus regulations (including time of day restrictions) to further minimize any noise impact.

5.10.2 No-Action Alternative

The No-Action Alternative would have no noise impact.

5.11 Land Use

5.11.1 Preferred Alternative

The Preferred Alternative would have no impact on land use.

5.11.2 No-Action Alternative

The No-Action Alternative would have no impact on land use.

5.12 Aesthetics

5.12.1 Preferred Alternative

The Preferred Alternative would have no impact on aesthetics. The NIH Architectural Design Review Board has reviewed and approved the current plans for the new bridge. This includes cast-in-place concrete parapets with stone masonry veneer. The stone finish will replicate a rustic ashlar pattern, similar to the existing bridge parapets. The natural stone, similar to the current bridge, will complement the existing context. Utility conduits currently hang on the

outside of both existing parapets and are highly visible and out of context. The utility conduits will be concealed by running them through the interior of both parapets on the new bridge.

5.12.2 No-Action Alternative

The No-Action Alternative would have no impact on aesthetics.

5.13 Ecological Resources

5.13.1 Preferred Alternative

No state or federally-listed rare, threatened, or endangered species are found within the project study area. The proposed bridge would span completely over the stream. However, instream work is required to remove portions of the concrete footings of the existing bridge. The footings would be removed to just below the stream surface. Instream work must be avoided during the period of March 1 through June 15 during any year.

The limit of disturbance for the bridge construction is identified on the plans. Currently proposed impacts to the stream would result in the requirement of the submission of a Joint Federal/State Application for the Alteration of Any Floodplain, Waterway, Tidal, or Nontidal Wetland in Maryland. However, because the work will result in less than 200 linear feet of impact and may be classified as a temporary impact, it may only require a letter of authorization from the Maryland Department of Environment and not a US Army Corps of Engineers permit.

Any tree impact would be coordinated directly with NIH. Both Stormwater Management and Erosion and Sediment Control are currently in conceptual plan and must be further designed as the project progresses. NIH will follow all applicable guidelines and regulations.

5.13.2 No-Action Alternative

The No-Action Alternative would have no impact on ecological resources.

5.14 Transportation

5.14.1 Preferred Alternative

The Preferred Alternative would allow the North Drive entrance to the NIH campus to remain open. Additionally it would allow a heavier load to safely traverse the stream, including fire truck access or egress as required. The North Drive entrance would be closed during the demolition of the existing bridge and the construction of the new bridge. During demolition and construction, traffic would be re-routed to other campus entrances on MD 355.

5.14.2 No-Action Alternative

The No-Action Alternative would result in the continued deterioration of the bridge to the point of permanent closure. Thus the North Drive entrance to the NIH campus would also be closed.

5.15 Historic Resources

5.15.1 Preferred Alternative

The North Drive Bridge is within the viewshed of only two buildings, neither of which are on the Maryland Inventory of Historic Places or are listed or eligible for listing on the National Register of Historic Places. Building 33 was constructed in 2003 and MLP 10 is a multi-level parking garage constructed in 2005. At its closest point, the bridge is in excess of 500 feet away from Building 6 and is obstructed from its viewshed by Building 33. Therefore, none of the build alternatives are expected to have an impact on the viewshed.

Given previous disturbance from the institutional land use and continuous road maintenance within the APE, the proposed replacement of the bridge appears unlikely to affect significant archeological resources.

Through coordination, the Maryland Historical Trust has determined that this project will have no adverse effect on historic properties.

5.15.2 No-Action Alternative

The No-Action Alternative would have no impact on historic resources.

5.16 Utilities and Services

5.16.1 Preferred Alternative

The Preferred Alternative would preserve the current service and utility capability provided by the existing bridge.

5.16.2 No-Action Alternative

The No-Action Alternative may impact utilities and services. Continued deterioration of the bridge would result in road closure. North Drive would not be accessible to any vehicles, including service vehicles and emergency vehicles. Ultimately, in the unlikely event of collapse of the existing bridge, existing utility conduits on the bridge would be severed.

6. Cumulative Impacts

Cumulative impacts on the environment result from the incremental effect of an action when added to other past, present, and reasonably foreseeable future actions, regardless of what agency (Federal or non-Federal) or person undertakes such other actions. These effects can result from individually minor, but collectively significant actions taking place over a period of time (40 CFR 1508.7).

The NIH Bethesda Campus consists of research, support, and office facilities and open space on 310 acres of land. The campus uses the Federal Real Property Council's Performance Measures to evaluate its existing facilities regarding mission, utilization, cost, and condition. The project is not inconsistent with the NIH Bethesda Campus Comprehensive Draft Master Plan 2013. The proposed project will not provide new access to any new development areas and will have no significant impacts. Additionally, the Maryland State Highway Administration coordinates with adjacent property owners on any ongoing or planned improvements to its facilities, such as MD 355. Such improvements must not be inconsistent with local plans. Therefore, secondary impacts are not anticipated as a result of the North Drive Bridge project.

7. Public Involvement

The final plans for the proposed North Drive Bridge will be reviewed and approved by the NIH Architectural Review Board. Additionally, this Environmental Assessment will be submitted to the Maryland State Clearinghouse and will be available for public review and comment.

Appendix



DEPARTMENT OF HEALTH & HUMAN SERVICES

RECEIVED
JAN 29 2013

Public Health Service

F
NIH
JES/EJZ

BY: _____

National Institutes of Health
Bethesda, Maryland 20892

www.nih.gov

January 24, 2013

201300379

The Maryland Historical Trust has determined that this undertaking will have no adverse effect on historic properties.
Beth Col Date 2/26/13

Mr. J. Rodney Little
State Historic Preservation Officer
Maryland Historical Trust
100 Community Place
Crownsville, MD 21032-2023

Subject: Bridge Replacement
North Drive Bridge over unnamed tributary to Rock Creek
Montgomery County
USGS Kensington 7.5' Quadrangle

Dear Mr. Little:

The National Institutes of Health (NIH) respectfully informs the Maryland Historical Trust (MHT) of our finding that there will be no historic properties affected by the proposed replacement of the North Drive Bridge on the NIH campus in Bethesda, Maryland (**Attachment 1**).

The bridge is located on North Drive over an unnamed tributary to Rock Creek, 150 feet west of MD 355. North Drive is a one-way two-lane road that leads west from southbound MD 355 to the campus. Usually open only to employee traffic and only between 6:00 AM and 10:00 AM daily, North Drive is currently closed to traffic pending environmental and engineering studies of the bridge. A *Structural Condition Assessment for North Drive Bridge*, (Maiden & Associates, 2012) determined that the bridge cannot currently support an H-15 loading since two pier bents have already failed due to insufficient load carrying capacities. There is no load limitation sign posted at the bridge site but the southernmost lane will remain closed after the bridge studies are completed.

The existing bridge has a 25-foot 10-inch wide deck with 13-inch wide stone railings on each side for a total bridge width of 28 feet x 38 feet 7 inches long. The bridge has a three-span simply supported reinforced concrete deck on timber stringers, with three equal spans of 12 feet each. The bridge is supported by 15-inch diameter timber piers set atop a strip concrete footing and bent spill-through abutments at each end. The most recent rehabilitation was in 1976, when the original 2-inch thick timber plank deck was replaced with an 8-inch thick reinforced concrete deck placed into corrugated steel decking form.

The bridge has been damaged with visual evidence of broken beams and cracked decking. Because North Drive is not a state or local road and is not included in the National Highway System, a bridge sufficiency rating is not available. However, additional damage to the existing bridge includes the following.

- The cantilevered portion of the end timber girder (beam) has been permanently damaged because of loading beyond the maximum allowable compressive load capacity of the material.

M:35-009
2 n g
JES
2/5/13

Attachments: IABX 2/26/13 MKIND bridge replacement



MARYLAND
DEPARTMENT OF
NATURAL RESOURCES

Martin O'Malley, Governor
Anthony G. Brown, Lt. Governor
John R. Griffin, Secretary
Joseph P. Gill, Deputy Secretary

February 20, 2013

13-MIS-207

Valerie Nottingham
Division of Environmental Protection
Bldg 13/2S11
National Institutes of Health
Bethesda, MD 20892

Subject: Fisheries Information for the Proposed North Drive Bridge Replacement in Bethesda, Maryland.

Dear Ms. Nottingham,

The above referenced project has been reviewed to determine fisheries species in the vicinity of the proposed project. The proposed activities include the North Drive Bridge Replacement, at the North Drive entrance to the NIH facility, in Bethesda, Maryland.

Rock Creek (Washington Metropolitan River Basin) and tributaries near the site are classified as Use I streams (use). Generally, no instream work is permitted in Use I streams during the period of March 1 through June 15, inclusive, during any year.

No anadromous fish have been documented near the project site. These streams may support many resident fish species documented by our Maryland Biological Stream Survey. However, there are no Maryland Biological Stream Survey (MBSS) stations near the project location. MBSS data can be accessed via the MDDNR web page at <http://mdimap.towson.edu/streamhealth/>, allowing access to resource surveys in neighboring tributaries.

If you have further questions, please contact the Environmental Review Program at 410-260-8799.

Sincerely,

Ken Yetman
Environmental Review Program



Sustainable _____ Attainable

December 23, 2013

Ms. Valerie Nottingham
Deputy Director, Environmental Protection, ORF
National Institutes of Health
NIH, B13/2S11
900 Rockville Pike
Bethesda, MD 20892

STATE CLEARINGHOUSE RECOMMENDATION

State Application Identifier: MD20131028-0772

Applicant: National Institutes of Health

Project Description: Draft Environmental Assessment: The National Institutes of Health (NIH) is Proposing to Demolish and Replace the North Drive Bridge on the Main NIH Campus with a Two-Lane Bridge: Consider 2 alternatives, "Action" and "No Action"

Project Location: Montgomery County

Recommendation: Consistent with Qualifying Comment(s) and Contingent Upon Certain Action(s)

Dear Ms. Nottingham:

In accordance with Presidential Executive Order 12372 and Code of Maryland Regulation 34.02.01.04-.06, the State Clearinghouse has coordinated the intergovernmental review of the referenced project. This letter, with attachments, constitutes the State process review and recommendation based upon comments received to date. This recommendation is valid for a period of three years from the date of this letter.

Review comments were requested from the Maryland Department(s) of Natural Resources, Transportation, the Environment, Montgomery County, Maryland National Capital Parks and Planning Commission - Montgomery County, and the Maryland Department of Planning, including Maryland Historical Trust. As of this date, Maryland National Capital Parks and Planning Commission - Montgomery County has not submitted comments. This recommendation is contingent upon the applicant considering and addressing any problems or conditions that may be identified by their review. Any comments received will be forwarded.

Montgomery County and the Maryland Department of Planning, including the Maryland Historical Trust found this project to be consistent with their plans, programs, and objectives.

The Maryland Historical Trust has determined that the project will have "no effect" on historic properties and that the federal and/or State historic preservation requirements have been met.

Martin O'Malley, Governor
Anthony G. Brown, Lt. Governor

Richard Eberhart Hall, AICP, Secretary
Amanda Stakem Conn, Esq., Deputy Secretary

The Maryland Department(s) of Natural Resources and Environment found this project to be generally consistent with their plans, programs, and objectives, but included certain qualifying comments summarized below.

DNR-1 { The Maryland Department of Natural Resources stated that the Seasonal restrictions of in-stream work described in EA as well as proper sediment control BMPs should mitigate impacts to stream.

The Maryland Department of Environment addressed the following issues:

MDE-1 { 1. Construction, renovation and/or demolition of buildings and roadways must be performed in conformance with State regulations pertaining to "Particulate Matter from Materials Handling and Construction" (COMAR 26.11.06.03D), requiring that during any construction and/or demolition work, reasonable precaution must be taken to prevent particulate matter, such as fugitive dust, from becoming airborne.

2. If soil contamination is present, a permit for soil remediation is required from MDE's Air and Radiation Management Administration. Please contact the New Source Permits Division, Air and Radiation Management Administration at (410) 537-3230 to learn about the State's requirements for these permits.

MDE-2 { 3. If a project receives federal funding, approvals and/or permits, and will be located in a nonattainment area or maintenance area for ozone or carbon monoxide, the applicant should determine whether emissions from the project will exceed the thresholds identified in the federal rule on general conformity. If the project emissions will be greater than 25 tons per year, contact Brian Hug, Air and Radiation Management Administration, at (410) 537-4125 for further information regarding threshold limits.

MDE-3 { 4. The applicant should be advised that no cutback asphalt should be used during the months of June, July and August.

MDE-4 { 5. Development should be concentrated in suitable areas such as existing or planned population centers as identified in a county's comprehensive plan.

6. Any above ground or underground petroleum storage tanks, which may be utilized, must be installed and maintained in accordance with applicable State and federal laws and regulations. Underground storage tanks must be registered and the installation must be conducted and performed by a contractor certified to install underground storage tanks by the Land Management Administration in accordance with COMAR 26.10. Contact the Oil Control Program at (410) 537-3442 for additional information.

7. Additional information from the Science Service Administration is enclosed (see attachment).

NIH RESPONSE to DNR

DNR-1

BMPs will be utilized and time of year restrictions for instream work will be followed.

NIH RESPONSES to MDE

MDE-1

Demolition of the existing bridge and construction of the new bridge will conform with State regulations pertaining to "Particulate Matter from Materials Handling and Construction" (COMAR 26.11.06.03D).

MDE-2

Montgomery County is classified as a moderate nonattainment area for 8-hour O3, a nonattainment area for annual PM2.5, and a maintenance area for 1-hour and 8-hour CO. Montgomery County is classified as being in attainment for all other criteria air pollutants and averaging times. Air analyses are not warranted since the proposed project does not add any traffic capacity.

MDE-3

No cutback asphalt will be used from June 1 through August 31, inclusive.

MDE-4

The proposed project is located within a Montgomery County Certified Priority Funding Area. Additionally, this project is a minor system preservation and safety project and as such is not subject to the restrictions of the 1997 Smart Growth Priority Funding Areas Act.

NIH RESPONSE to SHA

The Maryland Department of Transportation, State Highway Administration (SHA) stated that their finding(s) of consistency is/are contingent upon the applicant taking the action(s) summarized below.

SHA-1 { While no traffic impact studies or access permits are necessary at this time, maintenance of traffic, especially concerning diversion of traffic to other NIH employee entrances on MD 355 (Rockville Pike) and MD 187 (Old Georgetown Road) during construction and entrance closure will be subject to review and approval by SHA. Please coordinate with Mr. Steve Foster, Chief, Access Management, SHA, at 410-545-5601 or sfoster1@sha.state.md.us.

Maintenance of traffic may affect and be affected by SHA projects on both MD 355 (Rockville Pike) and MD 187. These Projects include:

- A project to construct MD 355 (Rockville Pike) intersection improvements from Woodmont Avenue/Glenbook Parkway to Wood Road/South Drive. Construction of this project is to begin in spring 2015, and it is anticipated to open to traffic in fall 2017. For further information, contact Mr. Armando Henriquez, Transportation Engineer, Highway Design Division, SHA, at 410-545-8876 or via email at ahenriquez@sha.state.md.us.
- A project to relocate MD 355 (Rockville Pike) utilities at Cedar Lane/West Cedar Lane and at Jones Bridge Road/Center Drive and to construct MD 355 (Rockville Pike) dynamic lane controls at Cedar Lane/West Cedar Lane. Construction of this project is underway, and it is anticipated to open to traffic in late 2013. For further information, contact Ms. Christina Minkler, Transportation Engineer, Highway Design Division, SHA, at 410-545-8050, via email cminkler@sha.state.md.us.
- A project to construct MD 355 (Rockville Pike) intersection improvements at Cedar Lane/West Cedar Lane. Right-of-way acquisition is underway, construction is to begin in late 2013, and it is anticipated to open to traffic in summer 2015. For further information, contact Ms. Christina Minkler, Transportation Engineer, Highway Design Division, SHA, at 410-545-8050, via email cminkler@sha.state.md.us.
- A project to construct MD 187 (Old Georgetown Road) intersection improvements at Oakmont Avenue/West Cedar Lane. Right-of-way acquisition is underway, construction is to begin in summer 2014, and it is anticipated to open to traffic in summer 2015. This project may affect diverted traffic during MD 355 Crossing construction. For further information, contact Ms. Rebecca Lichtenstein, Transportation Design Engineer, Community Design, Highway Design Division, SHA, at 410-545-8911 or via email at rlichtenstein@sha.state.md.us.;

Any statement of consideration given to the comments(s) should be submitted to the approving authority, with a copy to the State Clearinghouse.

The State Application Identifier Number must be placed on any correspondence pertaining to this project. The State Clearinghouse must be kept informed if the approving authority cannot accommodate the recommendation.

SHA-1 Due to the condition of the bridge, the North Drive entrance to NIH has been closed to traffic as a safety precaution. Because this project occurs on the NIH campus, no maintenance of traffic is required on State roadways. Project staging will occur on the NIH campus.

Ms. Valerie Nottingham
December 23, 2013
Page 4
State Application Identifier: MD20131028-0772

Please remember, you must comply with all applicable state and local laws and regulations. If you need assistance or have questions, contact the State Clearinghouse staff person noted above at 410-767-4490 or through e-mail at nasrin.rahman@maryland.gov. **Also please complete the attached form and return it to the State Clearinghouse as soon as the status of the project is known. Any substitutions of this form must include the State Application Identifier Number. This will ensure that our files are complete.**

Thank you for your cooperation with the MIRC process.

Sincerely,


Linda C. Janey, J.D., Assistant Secretary

LCJ:NR
Enclosure(s)

cc:

Mark Radtke
Greg Golden - DNR
Amanda Degen - MDE

Melinda Gretsinger - MDOT
Greg Ossont - MTGM

Cathy Conlon - MNCPPCM
Peter Conrad - MDPL

Jason Dubow - MDPE
Beth Cole - MHT

13-0772_CRR.CLS.doc



Sustainable _____ Attainable

PROJECT STATUS FORM

Please complete this form and return it to the State Clearinghouse upon receipt of notification that the project has been approved or not approved by the approving authority.

TO: Maryland State Clearinghouse
 Maryland Department of Planning
 301 West Preston Street
 Room 1104
 Baltimore, MD 21201-2305

DATE: _____
 (Please fill in the date form completed)

FROM: _____
 (Name of person completing this form.)

PHONE: _____
 (Area Code & Phone number)

RE: State Application Identifier: MD20131028-0772
 Project Description: Draft Environmental Assessment: The National Institutes of Health (NIH) is Proposing to Demolish and Replace the North Drive Bridge on the Main NIH Campus with a Two-Lane Bridge: Consider 2 alternatives, "Action" and "No Action"

PROJECT APPROVAL			
This project/plan was: <input type="checkbox"/> Approved <input type="checkbox"/> Approved with Modification <input type="checkbox"/> Disapproved			
Name of Approving Authority:		Date Approved:	
_____		_____	

FUNDING APPROVAL			
<i>The funding (if applicable) has been approved for the period of:</i>			
_____, 201__ to _____, 201__ as follows:			
Federal \$:	Local \$:	State \$:	Other \$:
_____	_____	_____	_____

OTHER
<input type="checkbox"/> Further comment or explanation is attached

Martin O'Malley, Governor
 Anthony G. Brown, Lt. Governor

Richard Eberhart Hall, AICP Secretary
 Amanda Stakem Conn, Esq., Deputy Secretary