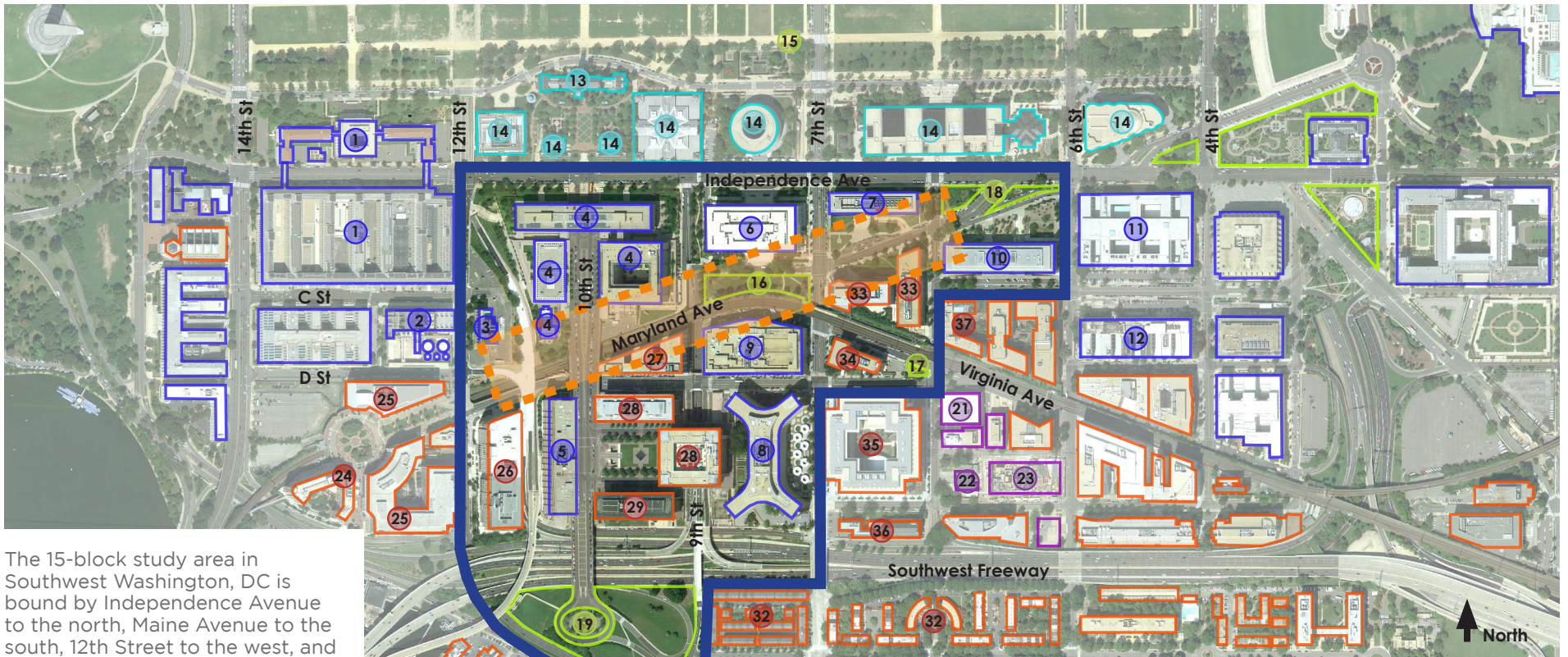


The SW Ecodistrict

PLAN ADDENDUM



DRAFT - MAY 2014



The 15-block study area in Southwest Washington, DC is bound by Independence Avenue to the north, Maine Avenue to the south, 12th Street to the west, and 4th Street to the east. This area comprises approximately 110 acres and includes privately and publicly owned land.

Unless otherwise noted, all streets are located in southwest Washington. The 'SW' suffix is presumed.

- | | | | |
|---|--|---|---|
| <ul style="list-style-type: none"> ① U.S. Department of Agriculture (Whitten Building) ② General Services Administration Central Utility Plant ③ Cotton Annex ④ U.S. Department of Energy (Forrestal Complex) ⑤ U.S. Postal Service ⑥ Federal Aviation Administration (Orville Wright Building) ⑦ Federal Aviation Administration (Wilbur Wright Building) | <ul style="list-style-type: none"> ⑧ US Department of Housing and Urban Development (Weaver Building) ⑨ General Services Administration (Regional Office Building) ⑩ U.S. Department of Education (Johnson Building) ⑪ Voice of America ⑫ US Departments of Health and Human Services / Education ⑬ Smithsonian Castle ⑭ Smithsonian Institution ⑮ The National Mall | <ul style="list-style-type: none"> ⑯ Reservation 113 ⑰ Reservation 115 ⑱ Proposed Eisenhower Memorial ⑲ Banneker Park ⑳ Jefferson Elementary School ㉑ DC Government Offices ㉒ DC Fire Department ㉓ DC Forensics Lab ㉔ Mandarin Oriental Hotel ㉕ Republic Properties ㉖ Potomac Center North, Inc. | <ul style="list-style-type: none"> ㉗ CIM Urban Reit, LLC ㉘ L'Enfant Plaza Hotel ㉙ L'Enfant Colony, LLC ㉚ PN Hoffman/The Wharf ㉛ NBL Associates ㉜ Private Residential ㉝ Boston Properties ㉞ 300 7th Street, LLC ㉟ Constitution Center ㊱ St. Dominic's Church ㊲ Federal Center Hotel Association |
|---|--|---|---|

Overview

The SW Ecodistrict Plan was accepted by the National Capital Planning Commission in January of 2013. This addendum includes technical information that informed the plan and additional guidance on urban design principles as a result of several follow-up studies since the plan's completion. The purpose of this addendum is to:

- › Provide technical information that informed the development of the SW Ecodistrict Plan.
- › Add clarity to the key public realm and development objectives of the SW Ecodistrict Plan.
- › Provide urban design principles that:
 - » Ensure new development is compatible with the Monumental Core of the Nation's Capital.
 - » Bridge the national and city interests as the area redevelops.
 - » Extend the civic character of the National Mall to Banneker Park.
 - » Create a site for a new museum or cultural use at Banneker Park.
 - » Preserve and enhance the importance of Maryland, Virginia, and Independence Avenues and 10th Street as significant elements of the Monumental Core and the Nation's Capital.

Use and Application

This appendix is a guiding document for future planning development decisions. It is not a regulatory document. It intended to be used to:

- › Inform future planning/redevelopment that GSA may pursue for the area.
- › Provide predictability and continuity for development in the area.
- › Inform the development and implementation of Smithsonian Master Plans.
- › Inform review of planning and development proposals.

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General Development Principles (SW Ecodistrict Plan pages 5 - 23)

HEIGHT AND MASSING

- › Building massing and design should reinforce the street wall and define L'Enfant¹ streets, landmark elements, view corridors and open spaces.
- › New building massing and design should complement surrounding buildings in scale, proportion, setback and alignment of horizontal and vertical elements.
- › Building height should ensure the prominence of national symbols such as the U.S. Capitol, Washington Monument, and the Smithsonian Castle when viewed from within the city and from panoramic viewpoints.
- › Architectural embellishments, such as towers and spires should not compete with the Smithsonian buildings or views of the National Mall, the Capitol, and Banneker Park.

BUILD-TO-LINES

- › Building design should form a consistent street wall at the property line. See build-to line exceptions for Independence Avenue (page 11) and Banneker Park (page 30).
- › Exceptions to the build-to-line may be provided for the provision of pedestrian entries and other outdoor features such as arcades, plazas, courtyards, outdoor seating and walk-up windows.

ROOFSCAPE

- › Roofscapes are critical visual elements to create successful buildings. Rooftop mechanical and telecom equipment should be integrated within the massing of the building. Antennas should be appropriately screened or hidden from view. Whenever possible, roof structures should be located so that they are not visible from the street. All roof structures should be attractively designed as integral parts of the overall building composition. Roof structures should maintain the same quality of materials, design interest, and architectural detail as other parts of the building façade.

FAÇADE COMPOSITION

- › Façades along primary streets should have a high level of design, materials, and articulation. The intent is to break down the scale of expansive buildings on a block to add variety, interest, and a pedestrian scale. This can be achieved by:
 - » Providing multiple openings
 - » Varying elevations, planes, and building materials
 - » Highlighting structural bays, building bases, and roof lines
- › New building massing and design should consider the design of adjacent existing or proposed buildings.

GROUND FLOOR

¹When referring to L'Enfant streets, the SW Ecodistrict Plan and Addendum include Virginia Avenue from Reservation 113 to Independence (even though it was technically not added until the Ellicott Plan of 1792).

- › The ground floor level design and scale of building façades should enhance the pedestrian experience by being visually interesting, transparent, and active.
- › Breaks in the ground floor for vehicular and service entries should be minimized.
- › Blank walls on the ground floor should be minimized. If continuous lengths of blank walls are unavoidable, the blank walls shall be articulated to provide interest and variety to the streetscape.
- › Entries should be oriented to a public street or park and should be prominent and visually distinctive from the rest of the façade with creative use of scale, materials, glazing, projecting or recessed forms, architectural details, color, and/or awnings.
- › Buildings on parcels adjacent to Reservation 113 should be designed to engage the park with prominent entries and ground floor uses.
- › No projections into the right-of-way such as unenclosed balconies should be permitted on Independence and Maryland Avenues.

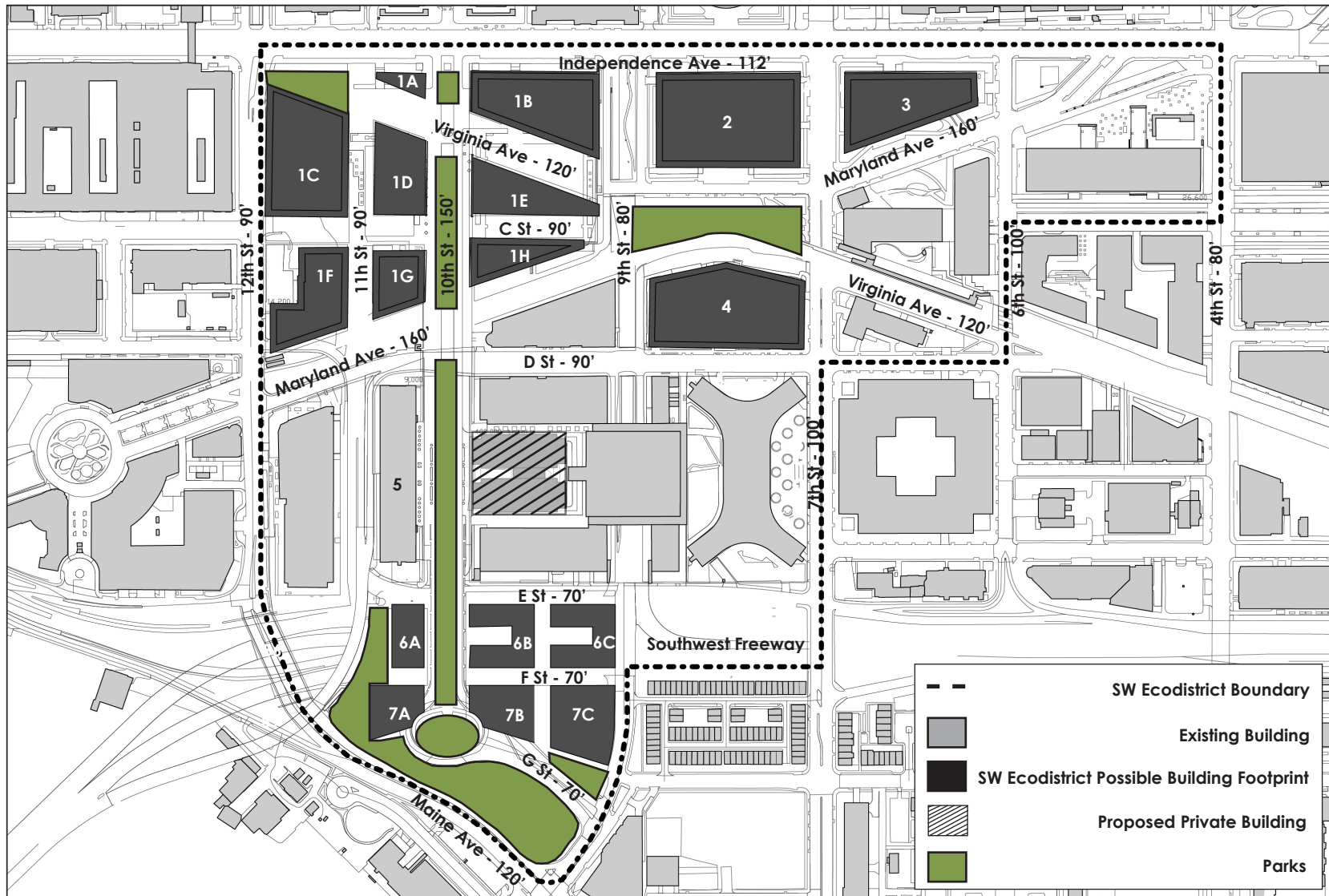
PUBLIC REALM

- › Use landscaping, including trees, shrubs, and ground cover plantings to: create defined outdoor spaces; reinforce important views and corridors; and establish active and intimate spaces.
- › Use landscaping to provide shade, beauty, perspective, massing and color to strengthen the identity of the area.
- › Design the public realm and infrastructure to foster a diverse and healthy urban ecology.
- › Hardscape elements should use the overall material selection of the National Mall and Federal Triangle; most are listed in the National Mall Road Streetscape Manual. These elements should take on their own character through combinations and finishes that create a unique vocabulary and sense of place for the SW Ecodistrict. These materials should be high-quality and durable.
- › Landscape elements should focus on native plants of the Mid-Atlantic region which are also tolerant of urban conditions. Plantings should include a complete palette of large canopy trees, understory trees, shrubs, perennials and groundcover. Above-ground elements such as roof and vertical gardens should be used in areas where ground-level plantings aren't feasible.
- › Incorporate landmark elements into the public realm such as commemorative walks and public art. These should not compete with views of

SIGNAGE, AWNINGS, AND PROJECTIONS ON INDEPENDENCE, MARYLAND, AND VIRGINIA AVENUES AND 10TH STREET

- › Signs should be distinctive, well-crafted and oriented toward the pedestrian.
- › No signs shall be placed above the second floor of a building.
- › No fluctuating, pulsating, or moving lights designed to change appearance or motion videos should be permitted.
- › Awnings should be designed to be compatible and harmonious with the architectural character and overall façade organization of the building.

THE HEIGHT OF BUILDINGS ACT | NATIONAL INTERESTS TO PROTECT



ASSUMPTIONS

1. The allowable Height per the Height of Buildings Act is determined by the width of the street in which the building fronts plus 20' up to 130'.
2. All of the parcels that are adjacent to the Maryland Avenue can achieve 130' in height from other adjacent streets (so with regard to building height, it does not matter if Maryland Avenue is built).
3. Buildings fronting 10th Street can take their height from 10th Street (even as it becomes a bridge south of Maryland Avenue). The rationale is that several existing buildings which were built during urban renewal already front (and measure their height) from 10th Street (DOE, USPS, L'Enfant Plaza).

THE HEIGHT OF BUILDINGS ACT | NATIONAL INTERESTS TO PROTECT

Parcel	Which streets(s) does the parcel front?	Allowed Height per the Height Act/ Zoning Regulations*	National Interests to Protect	Additional Guidance
1a	Independence Ave / 10 th Street / Virginia Ave	130'/TBD*	Views of the Smithsonian Campus, the Washington Monument, Banneker Park, and views from the National Mall.	Special consideration should be given to design, height and massing to protect views of the Smithsonian Campus, the Washington Monument, and views from the Mall.
1b	Independence Ave / 10 th Street / Virginia Ave	130'/TBD*	Views of the Smithsonian Campus, the Washington Monument, Banneker Park, and views from the National Mall.	Special consideration should be given to design, height and massing to protect views of the Smithsonian Campus, the Washington Monument, and views from the Mall.
1c	Virginia Ave / 12 th Street	130'/TBD*	Views of the Smithsonian Campus, the Washington Monument, Banneker Park, and views from the National Mall.	Special consideration should be given to design, height and massing to protect views of the Smithsonian Campus, the Washington Monument, and views from the Mall.
1d	Virginia Ave / 10 th Street	130'/TBD*	Views of the Washington Monument, Smithsonian Castle and Banneker Park.	Building design should respect views of the Washington Monument, Smithsonian Castle and Banneker Park.
1e	Virginia Ave / 10 th Street	130'/TBD*	Views of the Washington Monument, Smithsonian Castle, and Banneker Park. Reservation 113.	Building design should engage Reservation 113 and have an entrance that fronts the park. Building design should respect views of the Washington Monument, Smithsonian Castle and Banneker Park.
1f	11 th Street / 12 th Street/ Maryland Ave	130'/TBD*	Views of the Capitol.	Building design should respect views of the Washington Monument, the Smithsonian Castle, and Banneker Park.
1g	10 th Street / Maryland Ave	130'/TBD*	Views of the Banneker Park, the Smithsonian Castle, and the Capitol.	Building design should respect views of the Capitol, the Smithsonian Castle, and Banneker Park.
1h	10 th Street / Maryland Ave	130'/TBD*	Views of Banneker Park, the Smithsonian Castle and the Capitol.	Building design should engage Reservation 113 and have an entrance that fronts the park. Building design should respect views of the Capitol and the Smithsonian Castle.
2	Independence Ave / Reservation 113	130'/TBD*	Views of the Smithsonian Campus and from the National Mall. Reservation 113.	Special consideration should be given to design, height, and massing to protect views of the Smithsonian Campus and views from the Mall. Building design should engage Reservation 113 and have an entrance that fronts the park.
3	Independence Ave/7 th /Maryland Ave	130'/TBD*	Views of the Capitol, the Smithsonian Campus and from the National Mall. Reservation 113.	Special consideration should be given to design, height, and massing to protect views of the Smithsonian Campus and from the Mall.
4	Reservation 113 (Maryland Ave) / D Street / 7 th Street	120'/TBD*	Reservation 113	Building design should engage Reservation 113 and have an entrance that fronts the park.
5	10 th Street	N/A (street level uses)	Views of the Smithsonian Castle/Banneker Park	Street-level retail, educational, and cultural uses along the USPS building's 10 th Street frontage should not impact the lobby on the ground floor.
6a	10 th Street	TBD**	Views of the Smithsonian Castle and Banneker Park.	Building design should respect views of Banneker Park.
6b	10 th Street	TBD**	Views of the Smithsonian Castle and Banneker Park.	Building design should respect views of Banneker Park
6c	F, G, and 9 th Streets	TBD**	N/A	Building massing should respect neighborhood development.
7a	Banneker Park	TBD**	New museum/commemorative work on National Park Service Property	Building massing should respect views of the Washington Monument from Maine Avenue. The building should step down and setback to respect neighborhood development.
7b	Banneker Park	TBD**	New museum/commemorative work on National Park Service Property	The building should step down and setback to respect neighborhood development.
7c	Banneker Park	TBD**	New museum/commemorative work on National Park Service Property	The building should step down and setback to respect neighborhood development.

*The allowable height per zoning will be determined through the District's Zoning Update. It may result in maximum allowable heights that are more restrictive than what the Height of Building Act allows.

** Building heights need further analysis.

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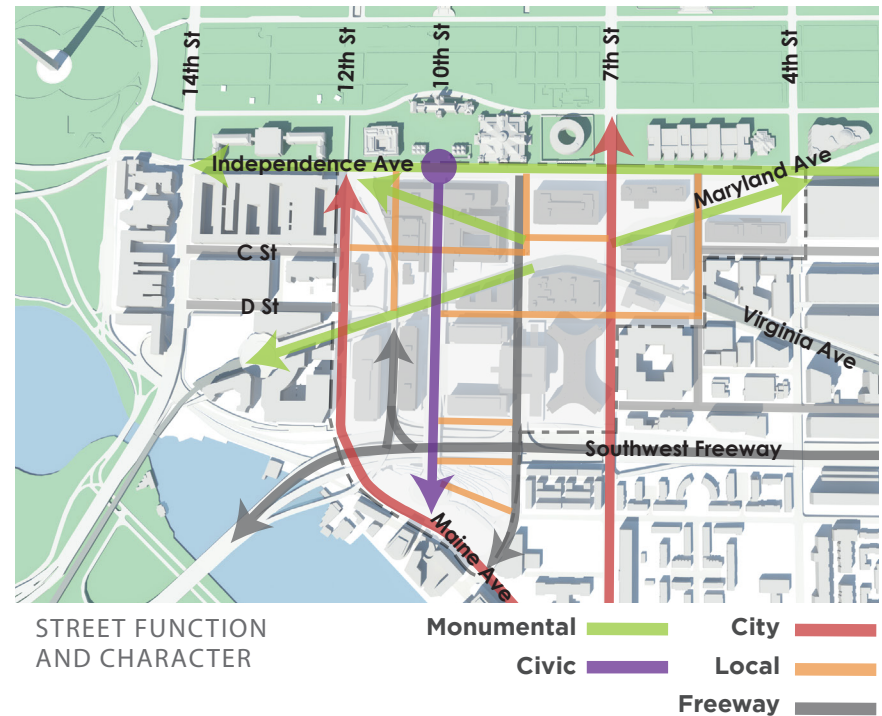
Street Character (SW Ecodistrict Plan pages 16 - 21)

The SW Ecodistrict Plan defines the character of each street within the plan boundary as monumental, civic, local or city as defined below.

ECODISTRICT PLAN STREET TYPE

- › Monumental: These streets are defined by their significant views towards prominent monuments and civic and governmental structures. The land use mix, programming and design of these streets should generate and prioritize pedestrian activity and respect the character and dignity of the Monumental Core of the nation’s capital. This classification would correlate with the District’s primary streets classification. Careful consideration should be given to the design of retail, service, and entertainment uses.
- › Civic: This typology refers to 10th Street. While prominent viewsheds also play a significant role on this street, emphasis is placed on the need for the street to accommodate a variety of outdoor events, both planned and impromptu, permanent and temporary. This street has a more park-like setting that encourages pedestrian activity. The land use mix, programming and design of these streets should encourage active ground floor uses that generate and prioritize pedestrian activity while respecting the character and dignity of the nation’s capital.
- › Local: Similar to the zoning code’s secondary street classification, these streets accommodate a greater mix of uses and day to day utilitarian uses like garage and ancillary residential entrances. These streets also provide more east-west and north -south neighborhood connections.

- › City: These streets should be designed for the pedestrian and accommodate efficient multi-modal cross-town vehicular traffic connection to freeways and other parts of the city. These streets allow a greater mix of uses.





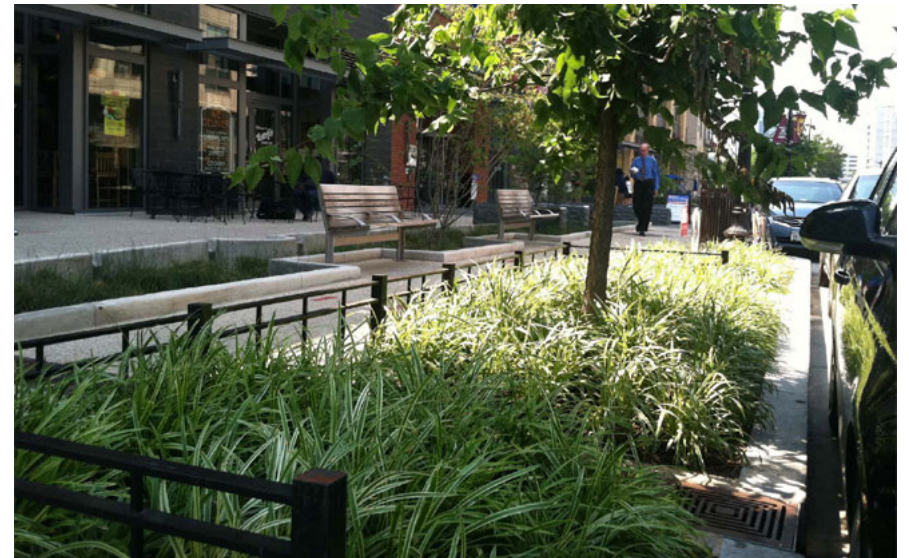
Civic Street Example: Las Ramblas, Barcelona



Monumental Street Example: Constitution Avenue, Federal Triangle, Washington, DC



City Street Example: 14th Street, NW, Washington, DC



Local Street Example: 1st Street, NE, NoMa, Washington, DC

RIGHT-OF-WAY DESIGN SUMMARY

Street Name	Right-of-Way Classification		Development Design Guidelines		Proposed Right-of-Way Design Characteristics													
	SW Ecodistrict	District of Columbia	Active Ground-Floor Requirements	Parking / Service / Loading Access	Right-of-Way			Build-to-Line	Vehicular Accomodation				Pedestrian Accommodation				Transit Facilities	Bike Facility
	Design Role and Character	Functional Classification Map	Guidelines for First 30' including: Use, First Floor Clear Height, Façade Transparency, Building Entries	Should it be allowed in the future along this frontage without an exception?	Historic	Existing	Proposed		Cartway (includes gutter)	Travel	Parking	Potential for Curb Extensions at Intersections	Sidewalks	Tree Box / Furnishing Area 7' Minimum (including curb)	Sidewalk Area (Pedestrian Clear Zone) 10' Minimum	Tenant Zone (Public Parking Area) Varies		
Diagonal Avenues																		
Independence Avenue	Monumental	Principal Arterial	Yes	No	112'	112'	To be Determined	Varies	80'	6 - 10' lanes	2 - 10' off peak lanes	Yes	2 - 20' Sidewalks	Exist	Exist	Exist	Bus	To be Determined
Potential variations on the design of right-of-way, with DDOT coordination, may include: intersection treatments, road diets, incorporation of curb extensions, accommodation of dedicated bike facilities, etc.																		
Virginia Avenue	Monumental	Not Applicable	Yes	No	120'	Section not Built	120'	0'	40'	2 - 12' Lanes	2 - 8' Lanes	Yes	2 - 40' Sidewalks	To be Determined	To be Determined	To be Determined		To be Determined
The design of the Right-of-Way, including dedicated width, will be evaluated with specific development/design proposals through a design review process																		
Maryland Avenue	Monumental	Local East of 12th, Collector East of 7th.	Yes	No	160'	Section not Built	160'	0'	40'	2 - 12' Lanes	2 - 8' Lanes	Yes	2-60' Areas = 2 - 14' Sidewalks at Building, 2 - 33' Landscape Zones, 2 - 13' Sidewalks at Roadway	7' Near Roadway	8' Near Building, 6' Near Roadway	6' Near Building		Sharrows
East-West Streets																		
C Street	Local	Local	Adjacent to Reservation 113	Yes - West of 9th	80'	90' (west of 9th Street)	90'	0'	40'	2 - 12' Lanes	2 - 8' Lanes	Yes	2 - 25' Areas	9'	10'	6'		To be Determined
The function, configuration and continuity of C Street between 10th Street and 9th Street may vary dependent on potential redevelopment in Federal Triangle South and may be evaluated through a design review process.																		
D Street	Local	Minor Arterial	Yes	No	70'	90'	90'	0'	56'	3 - 12' Lanes	2 - 10' Off Peak Lanes	Review with Streetcar	2 - 17' Areas	7'	10'		Bus, Potential Streetcar	Not with Streetcar
E Street	Local	Not Applicable		Yes	N/A	N/A	70' (Potential)	0'	To be Determined	2 - 11' Lanes	2 - 8' Lanes	Yes	2 - 13' Areas	7'	6'			To be Determined
F Street	Local	Not Applicable		Yes	N/A	N/A	70' (Potential)	0'	To be Determined	2 - 11' Lanes	2 - 8' Lanes	Yes	2 - 13' Areas	7'	6'			To be Determined
G Street	Local	Local	Yes	No	90'	N/A	70' (Potential - west of 9th Street)	0'	32'	2 - 11' Lanes	Not provided	Yes	2 - 19' Areas	7'	10'	2'	Bus	2 - 5' Bike Lanes
North-South Streets																		
12th Street	City	Local	Yes	No	85'	90'	90'	0'	60'	6 - 10' Lanes	Not provided	To be Determined	2-15' Areas	5'	10'		Bus	To be Determined
11th Street	Local / Freeway	Other Freeway and Expressway		Yes	N/A	90'	80'	0'	Varies - To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined
10th Street	Civic	Collector	Yes	No	85'	150'	150'	0'	2-20' = 40'	2 - 12' Lanes	2 - 8' Lanes	No - for flexibility for two way travel during events	2 - 24' Sidewalks, 52' Median	8'	10'	6'	Potential Bus	2 - 5' Cycle Tracks
9th Street	Local / Freeway	Other Freeway and Expressway /Collector		Yes	85'	80'	To be Determined	0'	Varies - To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined	To be Determined
7th Street	City	Minor Arterial	Yes	No	85'	100'	100'	0'	72'	6 - 12' Lanes	Not provided	Review with Streetcar	2 - 14' Areas	4'	10'		Bus, Potential Streetcar	Not with Streetcar
6th Street	Local	Collector		Yes	100'	100'	100'	0'	40'	2 - 10' Lanes	2 - 10' Lanes	To be Determined	2 - 30' Areas	7'	10'	13'		To be Determined
4th Street	Local	Minor Arterial	Yes	No	80'	80'	80'	0'	56'	4 - 10' Lanes	2 - 8' Lanes	To be Determined	2 - 12' Areas	6'	6'			To be Determined

3

Independence Avenue (SW Ecodistrict Plan pages 58 - 61)

Independence Avenue's adjacency to the National Mall and the Smithsonian campus and its connection to the U.S. Capitol characterize it as a monumental street, similar to the role that Constitution Avenue plays to the north side of the Mall. Independence Avenue is home to the Smithsonian's southern campus and a host of low profile federal buildings on the south side of the Avenue. These buildings are similar in height to the Smithsonian buildings and generally can not be seen over the rooflines of the Smithsonian buildings when standing on the Mall. The federal buildings also have a low-profile-profile when viewed in between the Smithsonian buildings from the Mall. The result when experiencing the Mall is that of a truly public space framed by trees and civic institutions.

- › Building height and massing along Independence Avenue should respect the character and experience of the public space of the National Mall and the scale of the Smithsonian Campus. New or redeveloped buildings along Independence Avenue should not compete with the Smithsonian buildings or privatize the character of the public space. Views of the tree line, the Smithsonian buildings, and open sky from the National Mall should be maximized. See explanation of the modeling that was developed for the SW Ecodistrict Plan (Addendum page. 12).
- › Land use on Independence Avenue should encourage street life activity and be compatible with the National Mall and its adjacent cultural institutions.
- › New buildings on Independence Avenue should consider stepping back a few feet at 35-45 feet in height to reinforce a pedestrian scale and a relationship with the Smithsonian campus.

- › Careful consideration should be given to the height, massing, and design of any new building on parcel C1 (see page 18) because of its location at 12th Street, Virginia Avenue, and Independence Avenue, and its close proximity to the U.S. Agriculture building and the Smithsonian Campus.
- › The build-to line on Independence Avenue should align with the reentrant northeast corner of the USDA building located at Independence Avenue and 12th Street (see diagram on page 13).
- › Landmark elements such as public art or commemorative works should be considered at important intersections to orient visitors to the area but should not compete with views of national symbols. Potential intersections include:
 - Independence Avenue and 10th Street
 - Independence Avenue and Virginia Avenue



Independence Avenue looking northeast towards the U.S. Capitol

SW ECODISTRICT PLAN ASSUMPTIONS FOR INDEPENDENCE AVENUE HEIGHT AND MASSING

Several factors contributed to the proposed height and massing of the development scenario in the SW Ecodistrict Plan. These include: the desire for a vibrant mixed-use neighborhood; GSA's asset management needs; the protection of views from the National Mall consisting of the Smithsonian buildings and the open sky; and the sense of symmetry of new development on Independence Avenue and with the Smithsonian campus.

While the existing building height and setbacks (which are similar to those of the Smithsonian buildings) would best protect the open sky views and public character of the National Mall, the plan recognized that the existing scale of development would not meet all of the plan's objectives.

The proposed development scenario sought to balance all of these objectives. The development scenario and physical model (below) show the height of buildings on the south side of Independence Avenue at a maximum of 130' with a setback from the property line and a step back further up, inclusive of all penthouses and mechanical equipment. While this scenario has a greater

impact on the national interests than what exists today, it maintains a sense of symmetry along Independence Avenue and a sense of openness from the Mall. The architectural features of the Smithsonian Castle and campus buildings are still prominent against the sky.

The SW Ecodistrict Plan/model assumed a setback of approximately 34' from the property line¹ to align with the northeast reentrant corner of the USDA building at 12th and Independence. The purpose of this setback is to maintain a similar setback as the Smithsonian buildings, and provide more ample sidewalk space and sunlight than what currently exists in front of the USDA building. As important, the 34' setback helps to mitigate the impact of the proposed height (130' inclusive of penthouse and mechanical equipment) on Independence Avenue as viewed from the National Mall.

Finally, the model shows a cornice line for the buildings on Independence Avenue at approximately 80-90' height to create symmetry with the scale of the Smithsonian buildings. The stepback shown is approximately 5'.



Model image of Independence Avenue looking east (left) and west (right) as proposed in the SW Ecodistrict Plan

¹The proposed 34' setback from the property line is approximate because the property lines along Independence Avenue vary by a few feet. The data source is DC GIS.

SETBACK FOR INDEPENDENCE AVENUE AS MODELED IN THE SW ECODISTRICT PLAN

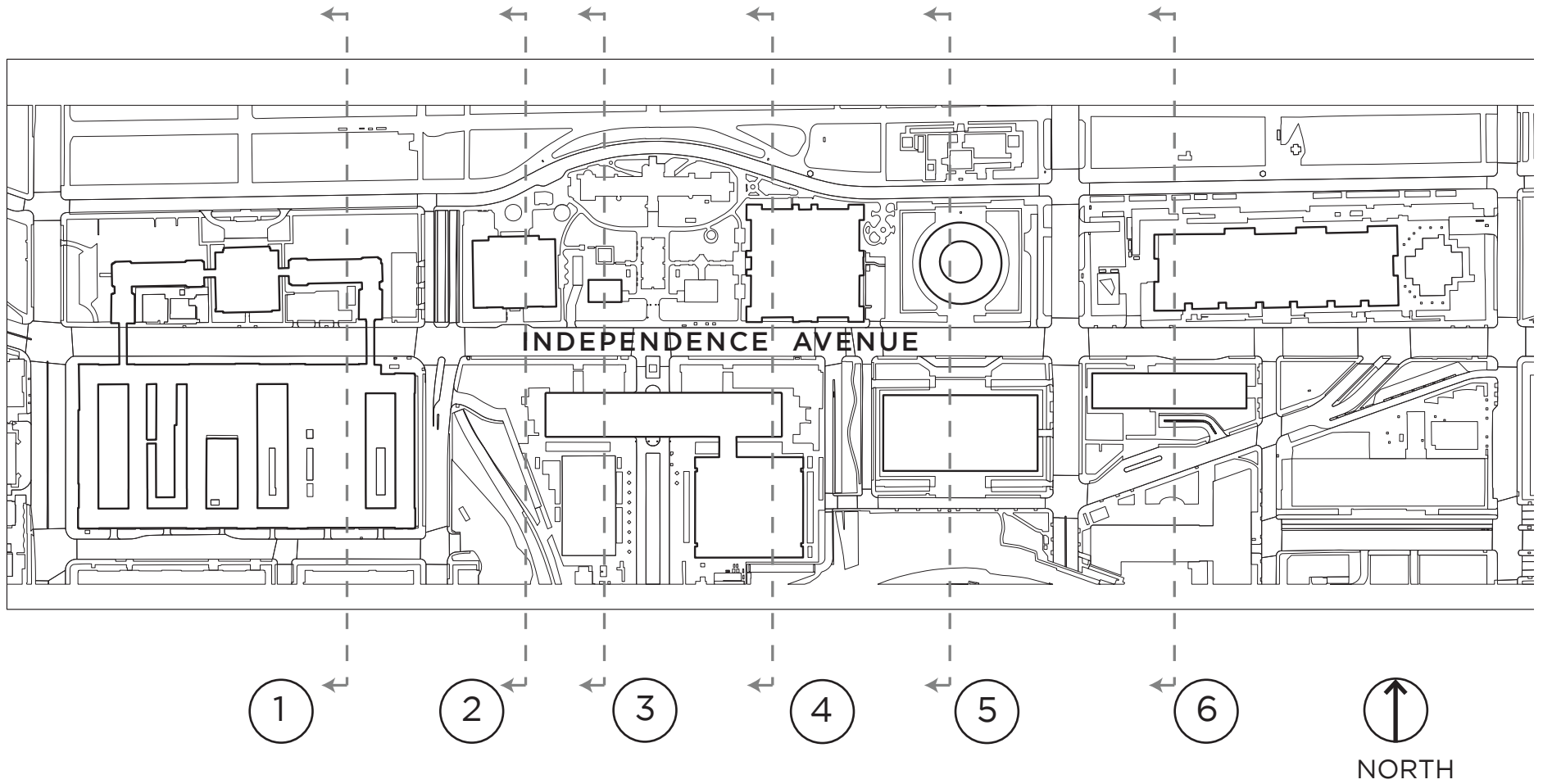
The existing height and setbacks vary by building on the south side of Independence Avenue; in general, the taller the building, the greater the setback. The Wilbur Wright building is the tallest building at 130' (with an additional penthouse) but it is set back from the property line approximately 91'. USDA only has a setback of 8' - 8" from the property line but is only 82' in height.

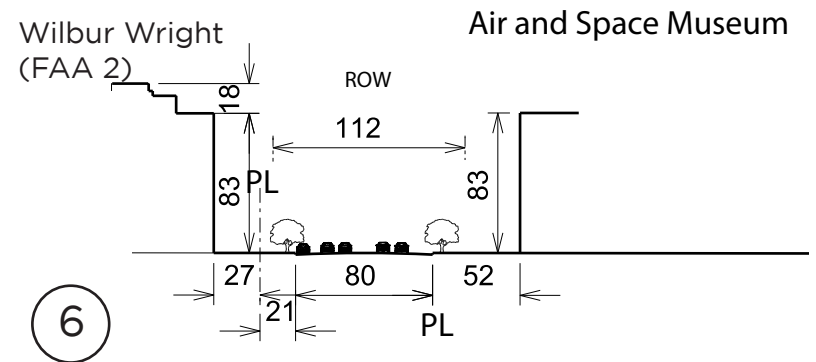
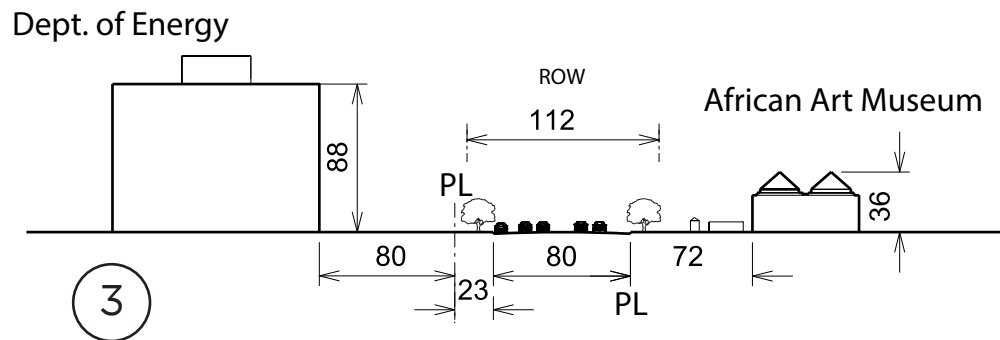
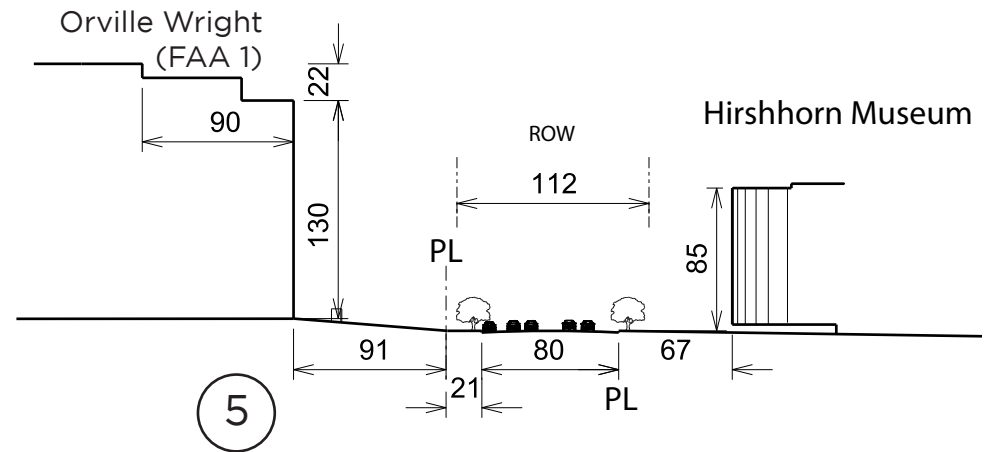
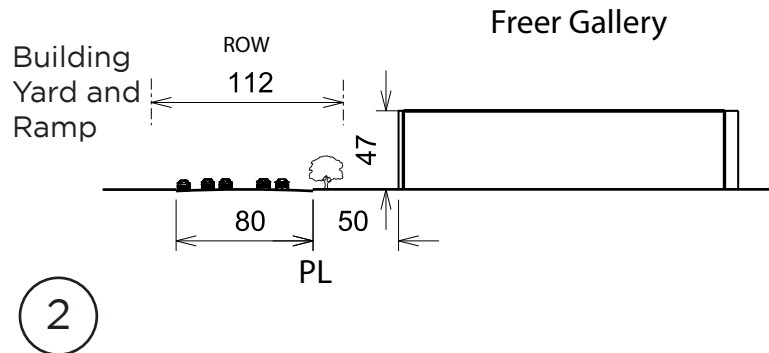
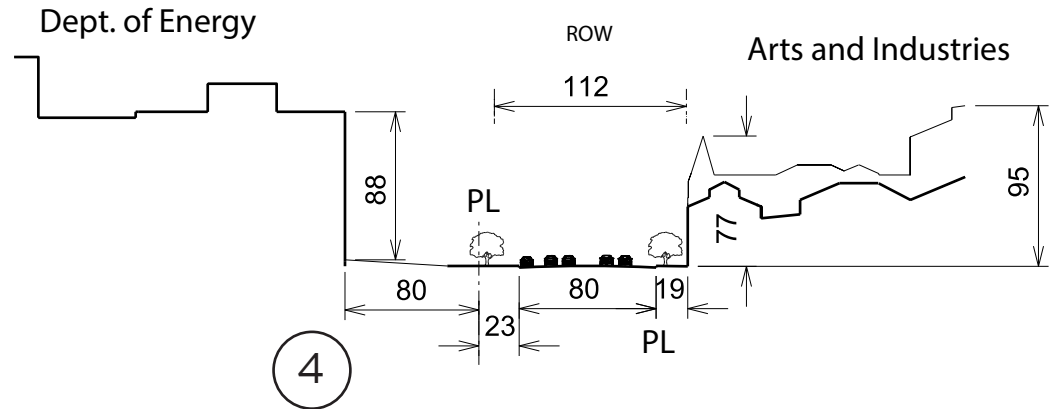
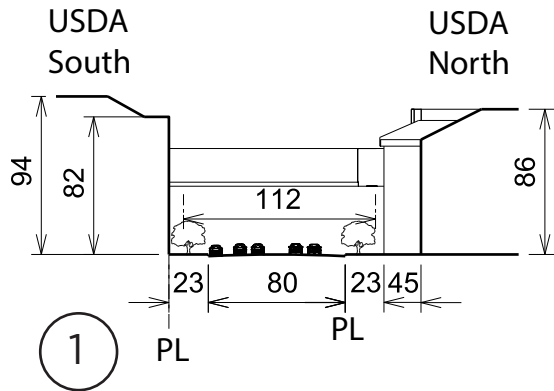
Building	Height	Setback from the property line
USDA	82'	8'8" from the property line along Independence Avenue/ 34' from the property line at the corner of 12 th and Independence Avenue
Forrestal Building (Dept. of Energy)	88'	80' from the property line
Orville Wright (FAA)	130'	91' from the property line
Wilbur Wright (FAA)	83'	27' from the property line

Source: DC GIS Data

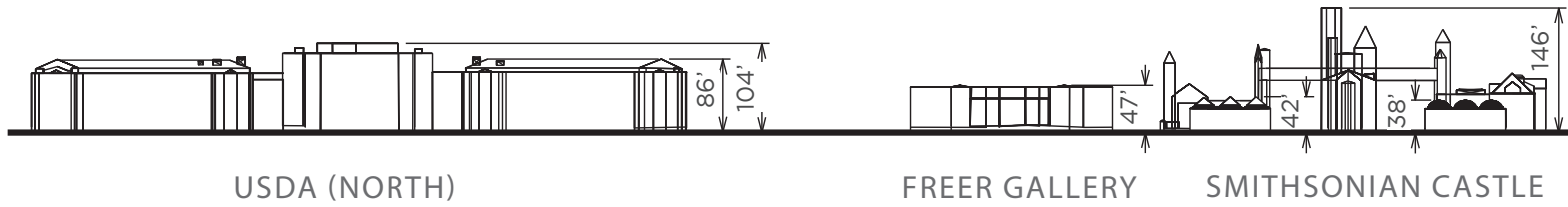


EXISTING HEIGHT AND SETBACK OF BUILDINGS ON INDEPENDENCE AVENUE

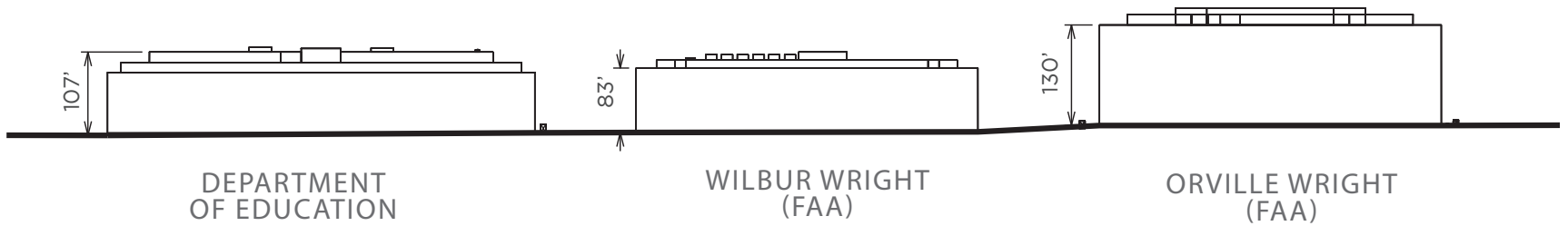


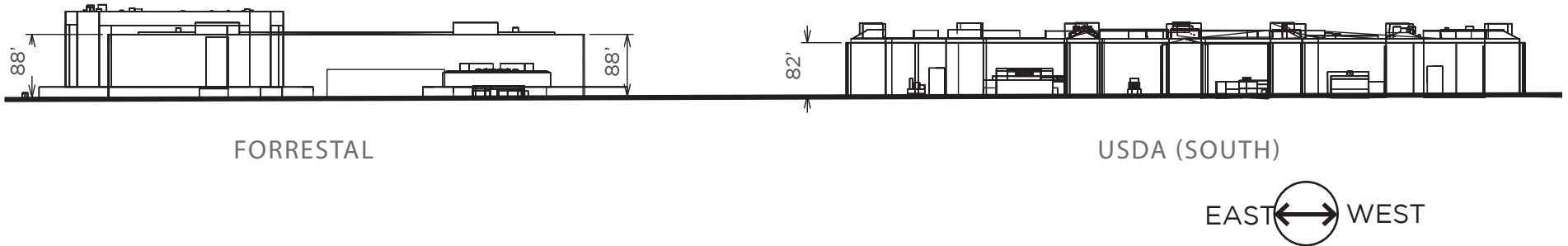
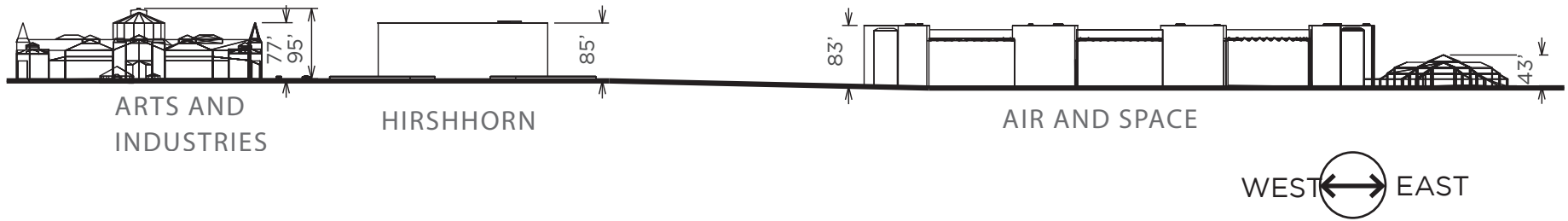


INDEPENDENCE AVENUE: EXISTING ELEVATION ALONG THE NORTH SIDE



INDEPENDENCE AVENUE: EXISTING ELEVATION ALONG THE SOUTH SIDE

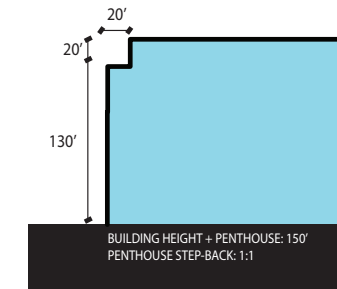
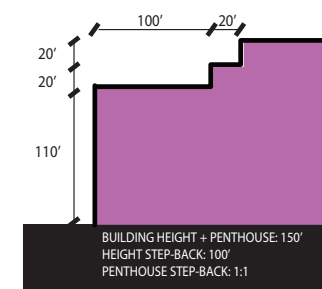
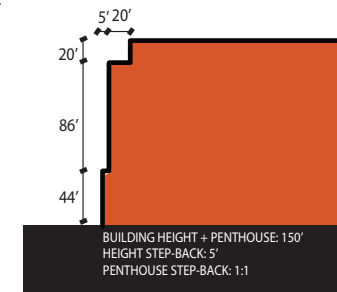
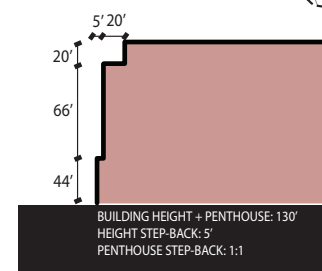
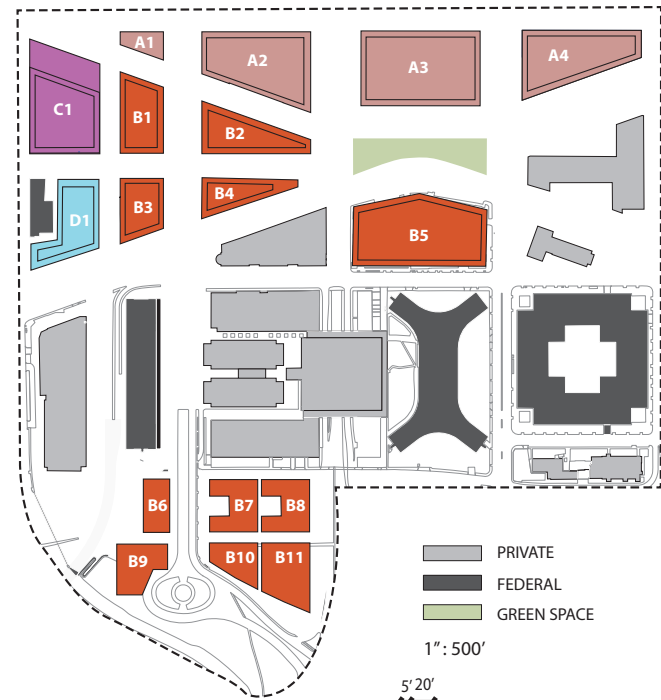
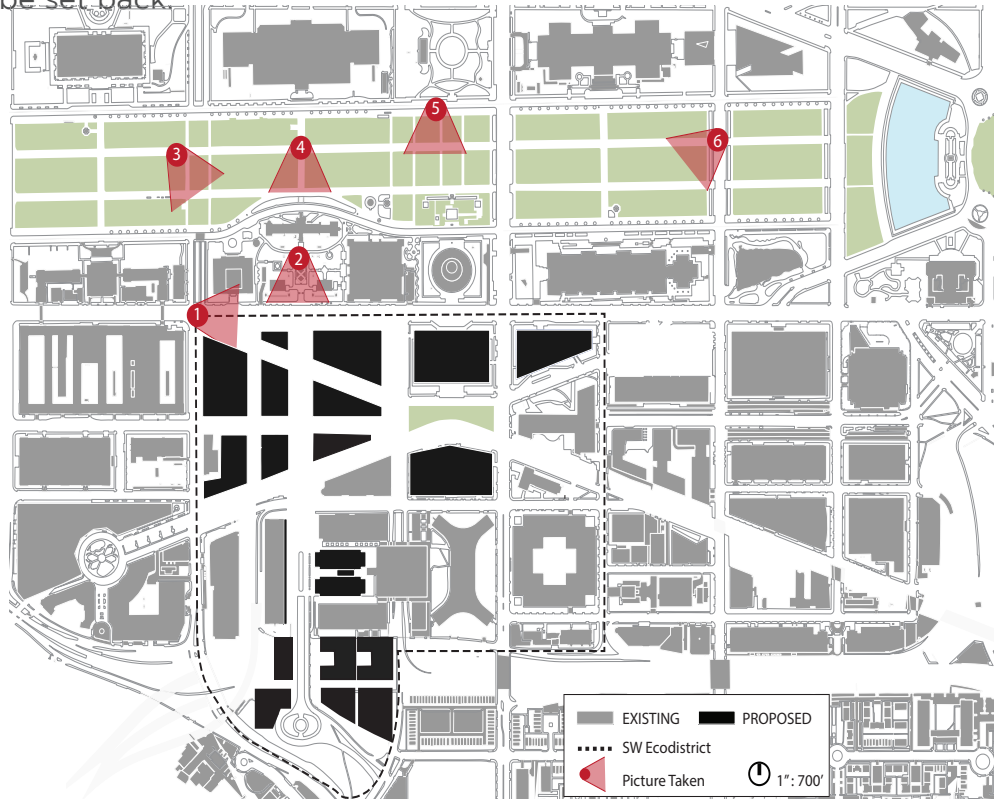




ADDITIONAL RENDERINGS OF THE SW ECODISTRICT PLAN ASSUMPTIONS

Since the development of the SW Ecodistrict Plan, NCPC has modeled several height and massing scenarios for the potential areas of redevelopment in the SW Ecodistrict. The modeling on pages 19 - 21 illustrate an additional height and massing alternative which could be considered when additional master planning occurs.

The additional modeling includes the same setback of 34' from the property line on Independence Avenue, but changes the maximum allowable height to 110' (not inclusive of a penthouse). It assumes a penthouse per the District's zoning regulations set back at a 1:1 ratio and up to 20' in height.¹ This would slightly reduce the development square footage from the scenario that was modeled in the plan but would have less of an impact on the national interests described above because the top 20' of the building (the penthouse) would be set back.

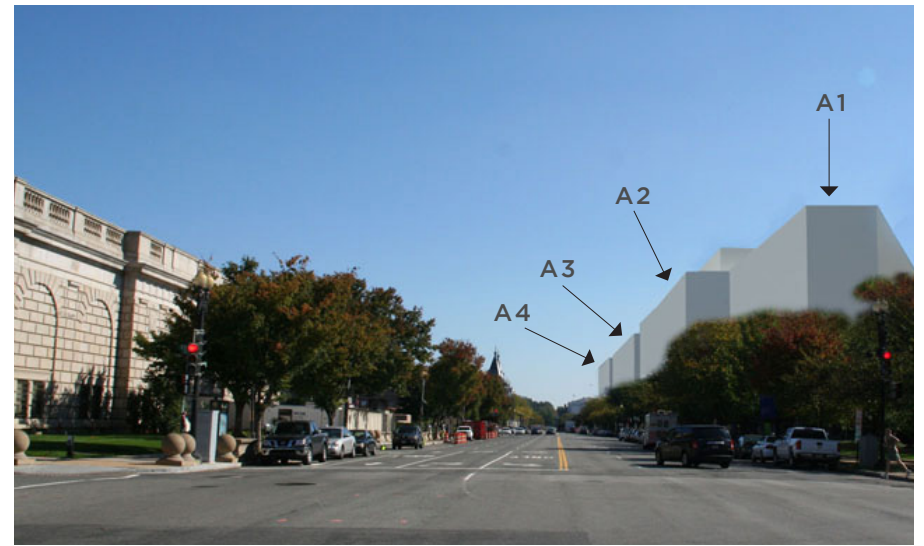


¹ NCPC assumed a penthouse of 20' in height contingent on pending legislation and potential changes to the zoning code.



VIEW 1 **EXISTING CONDITIONS**

Independence Avenue and 12th Street (looking east)



SW ECODISTRICT PLAN



VIEW 2 **EXISTING CONDITIONS**

Haupt Garden at Independence and 10th Street
(looking south on 10th Street toward Banneker Park)



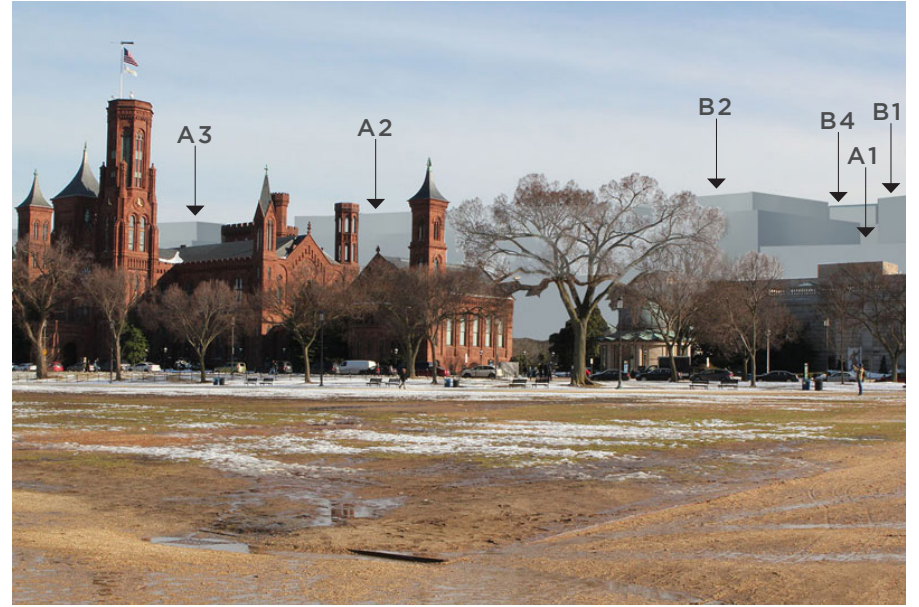
SW ECODISTRICT PLAN



VIEW 3

EXISTING CONDITIONS

The National Mall (at approximately 12th Street) looking southwest



SW ECODISTRICT PLAN



VIEW 4

EXISTING CONDITIONS

The National Mall (at approximately 10th Street) looking south

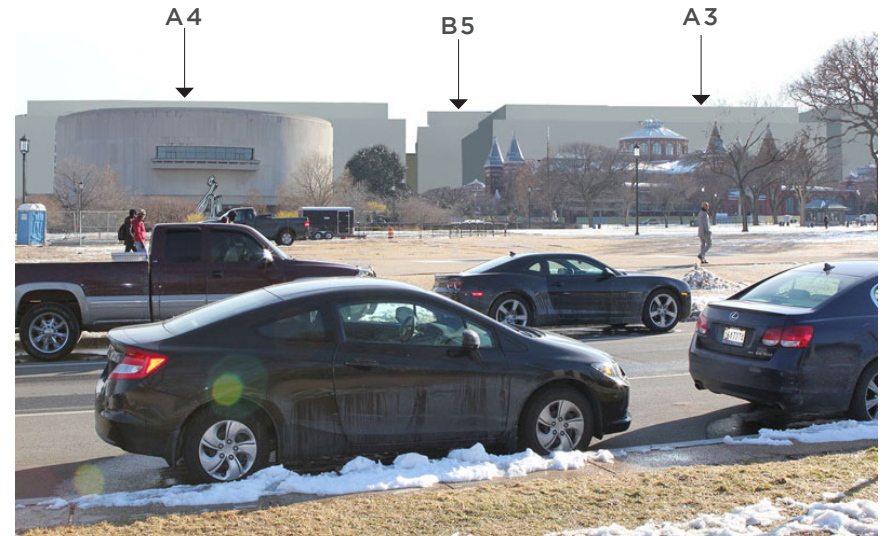


SW ECODISTRICT PLAN



VIEW 5 **EXISTING CONDITIONS**

The National Mall (at approximately 7th Street)
looking south



SW ECODISTRICT PLAN



VIEW 6 **EXISTING CONDITIONS**

The National Mall (at approximately 4th Street)
looking southeast.



SW ECODISTRICT PLAN

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Maryland Avenue (SW Ecodistrict Plan pages 70 - 77)

Maryland Avenue is a significant radial avenue that terminates at the U.S. Capitol. It is characterized as a monumental street and is defined by its significant views of the Capitol. It is also considered the sister street to Pennsylvania Avenue in the L'Enfant Plan. In the SW Ecodistrict, it will serve as a mixed-use corridor in this evolving neighborhood. As a result, programming and design along Maryland Avenue should respect the character and dignity of the Monumental Core of the nation's capital.

- › The design of buildings and streetscape on Maryland Avenue should maintain the prominence of the Capitol dome against the sky. Additional landmark elements should be considered at the Maryland Avenue and 10th Street intersection to orient the visitors to the area. They should not compete with views of the Capitol.



Top: Maryland Avenue looking east towards the U.S. Capitol.
Bottom: Maryland Avenue looking west towards Reservation 113.

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Virginia Avenue (SW Ecodistrict Plan pages 58 - 61)

Virginia Avenue is characterized as a monumental street. Historically it was defined by its iconic views of the Washington Monument. Its historic width in the L'Enfant Plan is 160'; however, the SW Ecodistrict Plan shows the right-of-way at 120' suggesting that because of the changed conditions, a reduction in width could be evaluated.

- › Building design, height, and massing along Virginia Avenue should respect the views of the Washington Monument and maintain its prominence as a symbol surrounded by open sky.
- › Virginia Avenue terminates at Triangle Park (see page 28) where there is an opportunity for a landmark commemorative element to be integrated into an urban plaza.
- › Curb cuts along Virginia Avenue may be necessary if a parcel also fronts Independence Avenue and/or 10th Street (where they should not be allowed).

Top: The Navy Memorial, adjacent to the Market Square development, serves as a successful example for the Triangle Park at Virginia and Independence Avenues.

Bottom: Recreating Virginia Avenue will restore a prominent view of the Washington Monument.



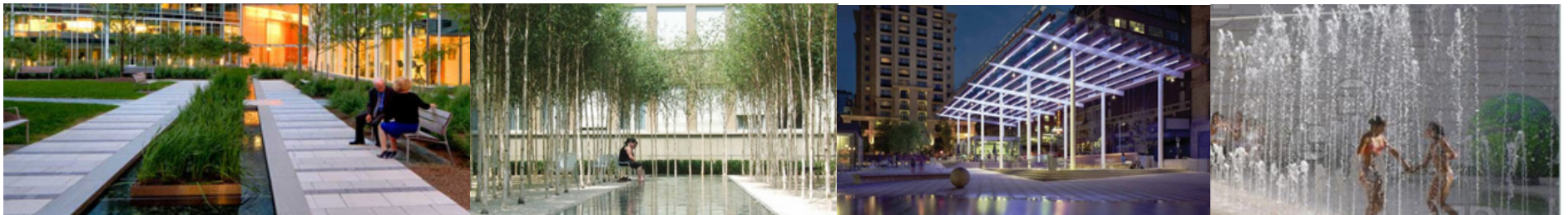
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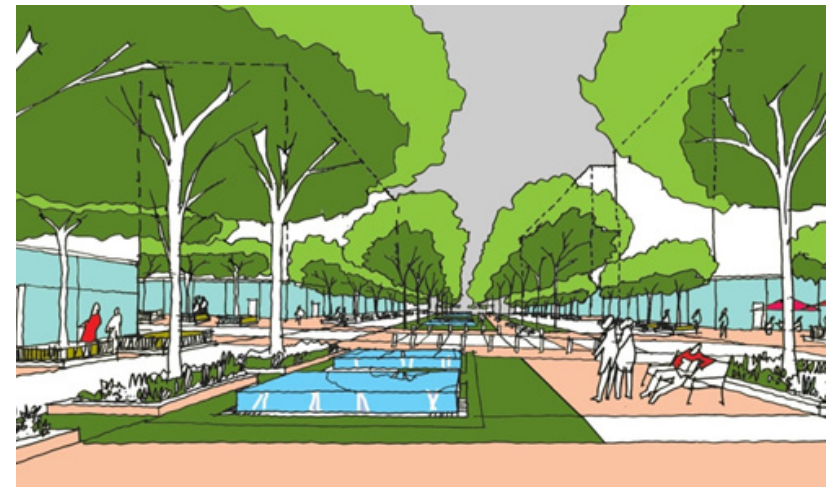
10th Street (SW Ecodistrict Plan pages 62 - 69)

10th Street, a half mile long urban garden promenade, is planned as an extension of the National Mall that connects the Monumental Core with Banneker Park and the waterfront. It provides opportunities for play, respite, public events/programs, commemoration and public art. Its character is a hybrid of hardscape, softscape and waterscape with an overall theme of sustainability.

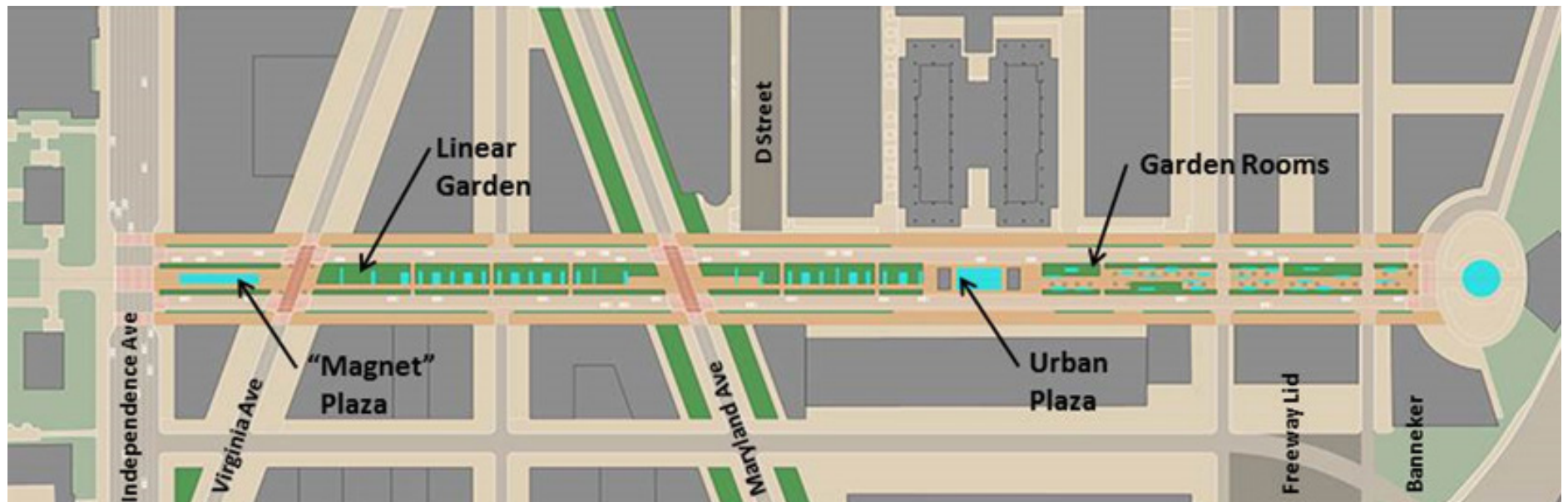
- › It is characterized as a civic street and prominent view corridor that also needs to accommodate active edges and a variety of outdoor events, both planned and impromptu, permanent and temporary. This street has a more park like setting that encourages activity.
- › 10th Street should be designed to function and read as the sustainability spine of the Ecodistrict, showcasing the district water and energy systems.

- › Building massing and design along 10th Street, should frame views of Banneker Park and the Smithsonian Castle.
- › New buildings on 10th Street should step back a few feet at 35 - 45 feet in height (similar to Independence Avenue) to create a continuous pedestrian scale.
- › Landmark elements such as public art and commemorative works should be considered at important intersections to orient visitors to the area. It should not compete with views of the U.S. Capitol. Potential intersections include:
 - Independence Avenue and 10th Street
 - Maryland Avenue and 10th Street
- › More detail regarding the 10th Street Programmatic Concept can be found at www.ncpc.gov/swecodistrict.





The proposed redevelopment of 10th Street provides for three distinct park features to be located in the median. This space will also accommodate special events and serve as an extension of the National Mall.



10TH STREET STORMWATER INFRASTRUCTURE STUDY

One of the most important aspects of 10th Street will be its ability to house a district water system that allows the area to meet its stormwater management and potable water reduction goals. In April of 2014, NCPC completed the SW Ecodistrict Stormwater Infrastructure Study which looks at the technical and financial feasibility of proposed district water system in the SW Ecodistrict Plan.

The purpose of the study is to:

- › Establish the role of the 10th Street corridor to store and convey captures and treated stormwater.
- › Determine the incremental value for stormwater capture and reuse between 10th Street corridor alternatives through transformational investments in the SW Ecodistrict.
- › Identify potential pathways to reduce the gap in capital and operation costs.

The study looks at:

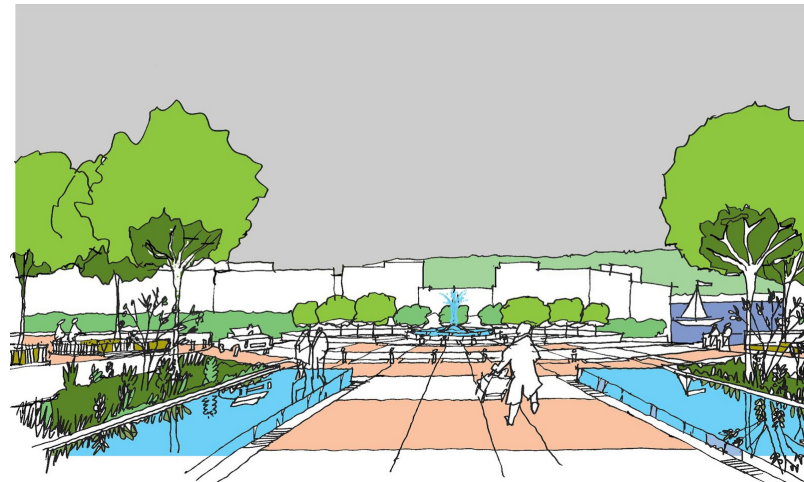
- › Climate and seasonal rainfall
- › Per capita water demand by use
- › Capital costs
- › Operational costs
- › Jurisdictional requirements
- › Public open space improvements

The findings of the study include:

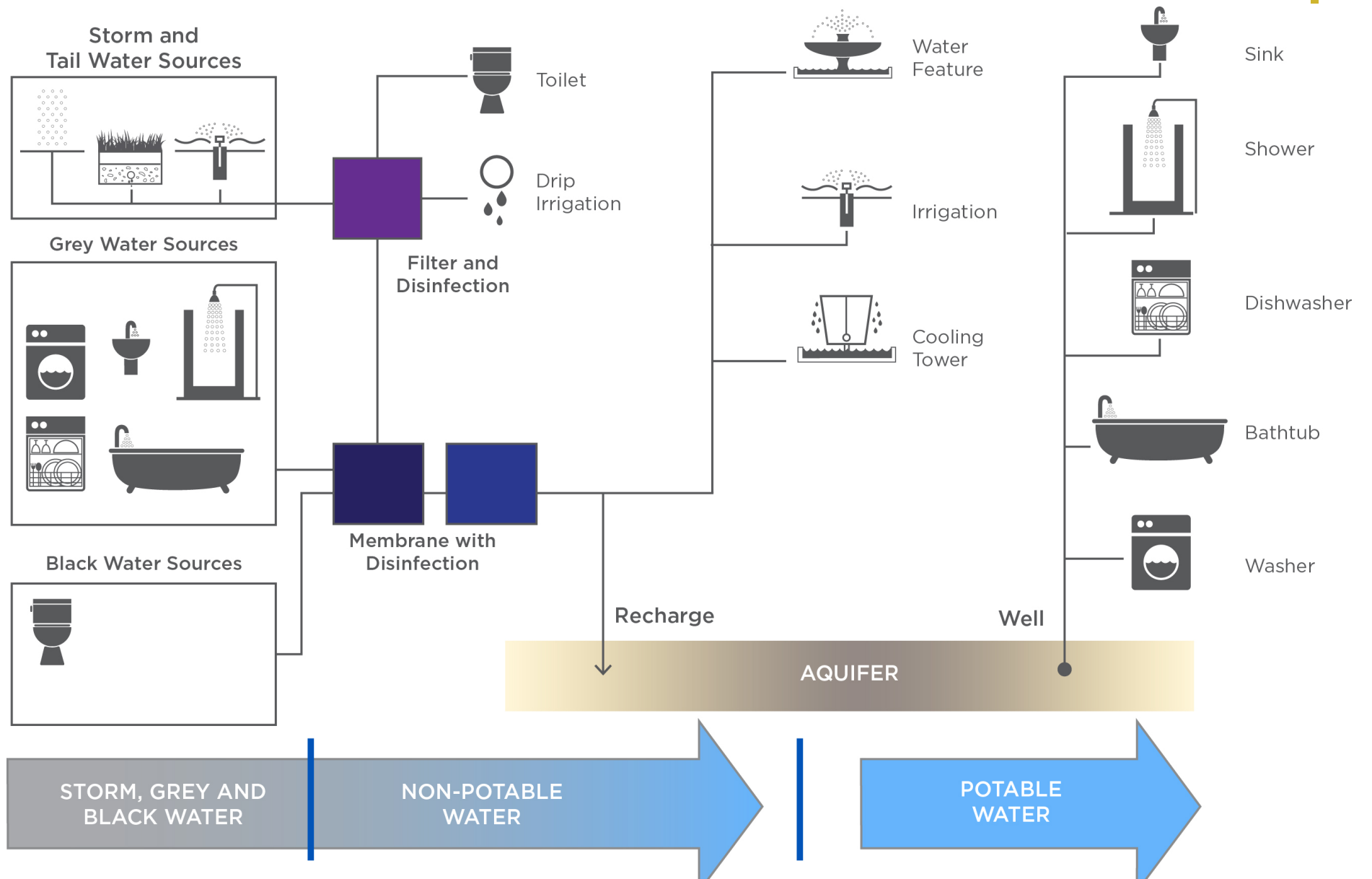
- › Not only is the district water system technologically feasible, it has a reasonable payback period.

- › At a minimum, collecting all the stormwater, reusing it for non-potable water use, and earning stormwater credits should be a given.
- › Treating stormwater and using well water to meet the ecodistrict's water needs is more complicated but still technologically feasible with a payback similar to a LEED Platinum building (yet it achieves so much more).
- › The most challenging aspect of the district water system is figuring out who will manage it.
- › If "aggregators" of stormwater credits are emerging in Washington DC in response to the new stormwater regulations, could they be a likely entity to manage this system?

To download the complete SW Ecodistrict Stormwater Infrastructure Study, go to: www.ncpc.gov/swecodistrict.



Water system improvements at a district scale will lead to the transformation of the environmental character of the ecodistrict.

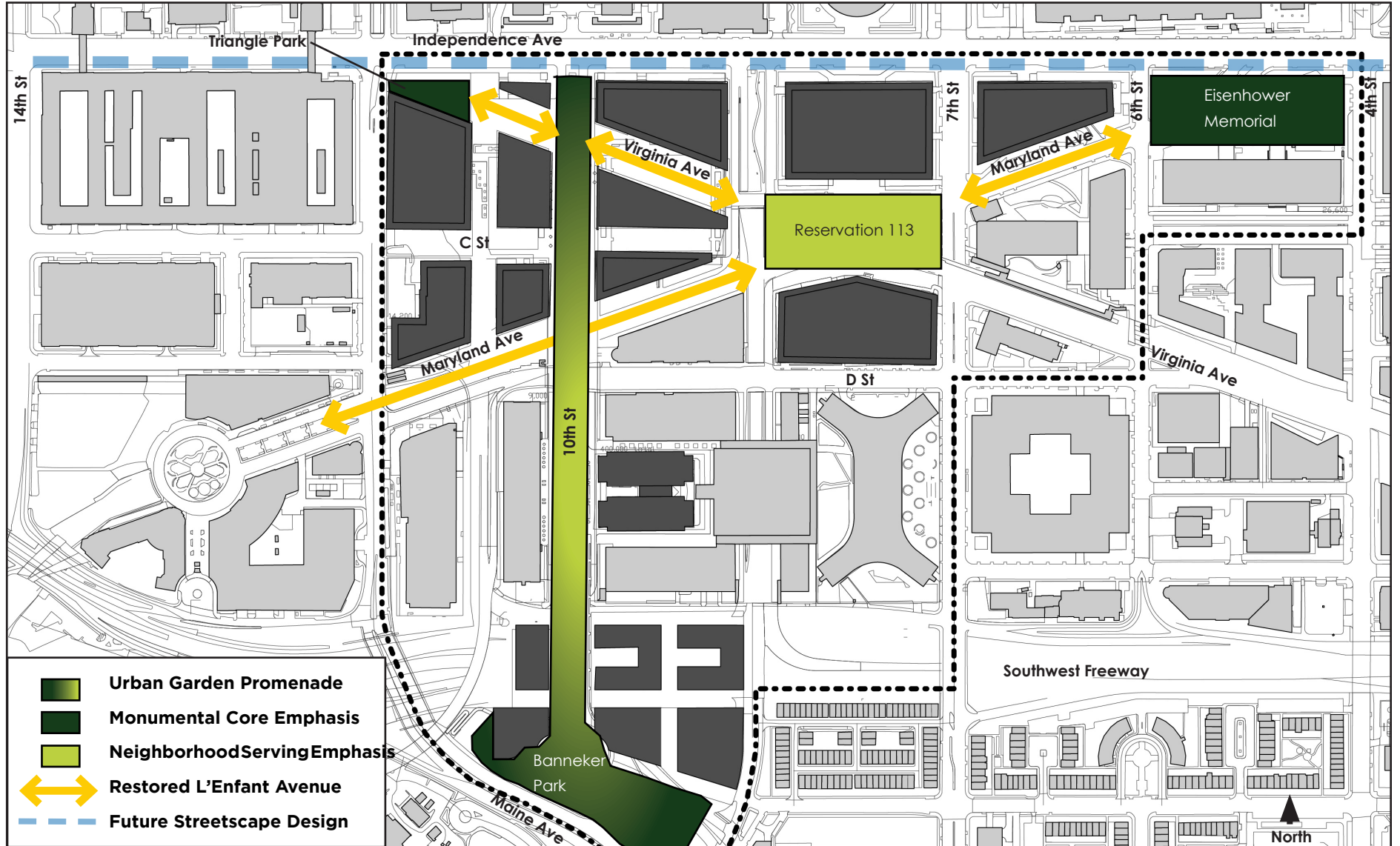


The Stormwater Study discusses the strategy above as the preferred approach for the various waters in the SW Ecodistrict.



Public Open Space (SW Ecodistrict Plan pages 6 - 9)

The parks are an important element in this neighborhood that bridges the local and capital city. Each type of park has a different emphasis and a variety of functions as discussed on the following pages.





Reservation 113 (SW Ecodistrict Plan pages 70 - 77)

Reservation 113 is envisioned as a prominent L’Enfant reservation where two great avenues convene. It is an outdoor room created by surrounding buildings.

› Reservation 113 should function as an important urban square in the L’Enfant Plan and as a significant commemorative site. It should also support the program of a quality neighborhood park.

- › Commercial activity should be encouraged around the park or in designated areas within the park.
- › The park design should reinforce the view of the U.S. Capitol.
- › Buildings on parcels adjacent to Reservation 113 should be designed to engage the park with prominent entries and ground floor uses.



Left: Bryant Park, New York City

Right: Conceptual rendering of Reservation 113 as shown in the SW Ecodistrict Plan.

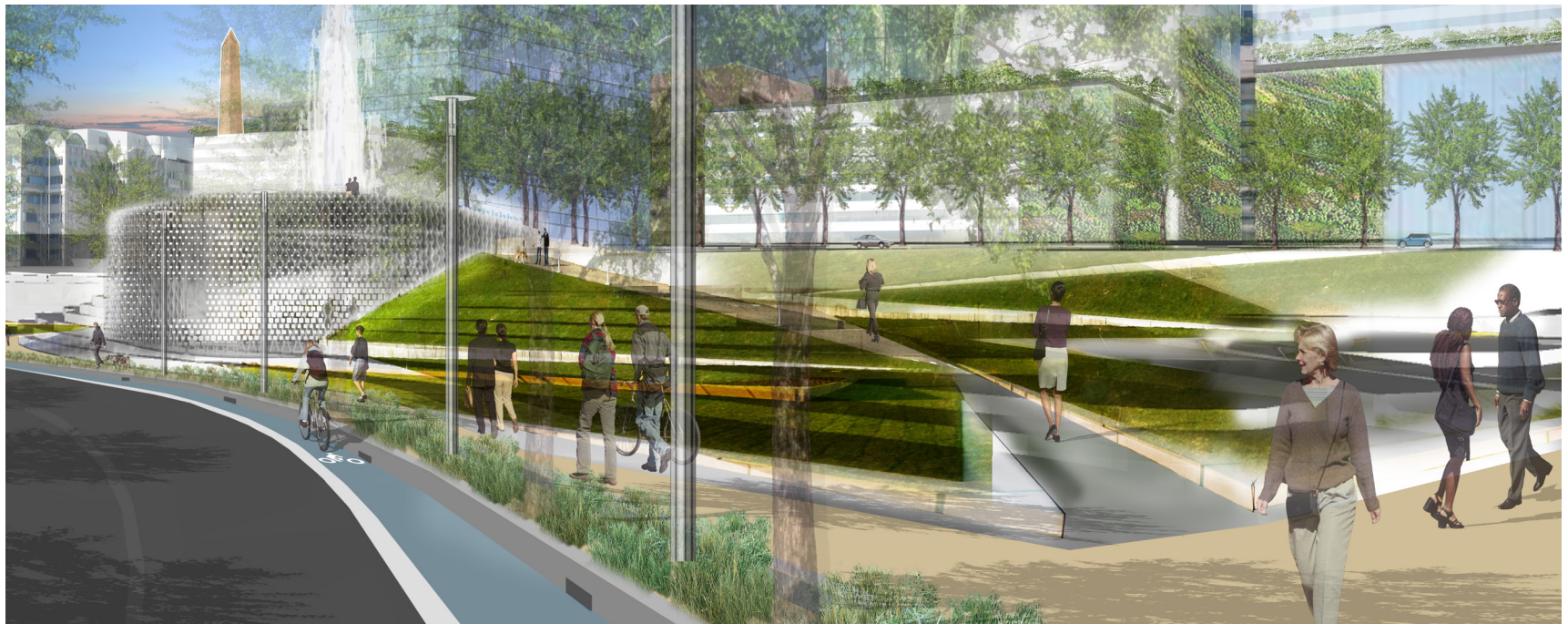


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Banneker Park (SW Ecodistrict Plan pages 68 - 69)

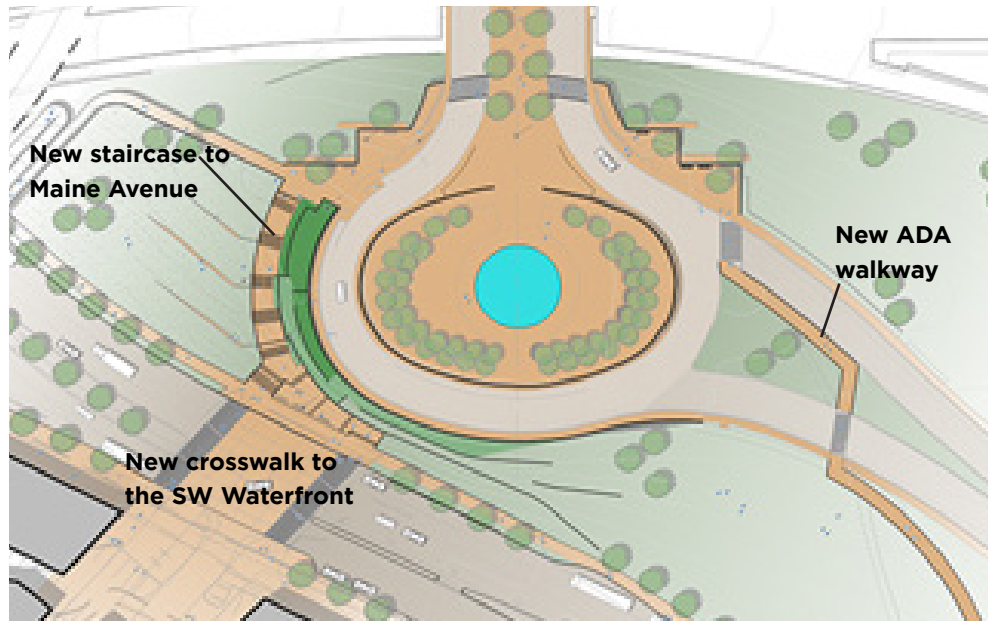
Banneker Park is as an extension of the Monumental Core and the National Mall. It has been identified in the Museums and Memorials Master Plan as a site for a nationally significant cultural facility. It will also serve as the gateway to the National Mall from the waterfront.

- › Locate, mass, and configure any new buildings to respect the location and scale of nearby residential development.
- › A modern, terraced landscape at Banneker Park is envisioned to enhance the park to provide a gateway to the National Mall.



Above: Looking up at Banneker Park from Maine Avenue

BANNEKER PARK INTERIM CONNECTION BETWEEN 10TH STREET AND THE WATERFRONT



- › In January of 2014 the National Capital Planning Commission commented favorably on a concept design for an interim connection at Banneker Park. More permanent access will be designed when a Master Plan is developed for the entire site. The purpose of the Banneker connection is to provide a safe, functional and aesthetically pleasing pedestrian connection that links Banneker Overlook with the new mixed-use development project on the waterfront called “The Wharf”. The Wharf project developers are required to build this connection as part of the approval of their planned unit development proposal. The proposed design includes a new stairway, ADA access, and minor upgrades to the landscape.

Below: The interim connection at Banneker Park is a key element of the SW Ecodistrict’s public realm.



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Triangle Park (SW Ecodistrict Plan pages 58 - 61)

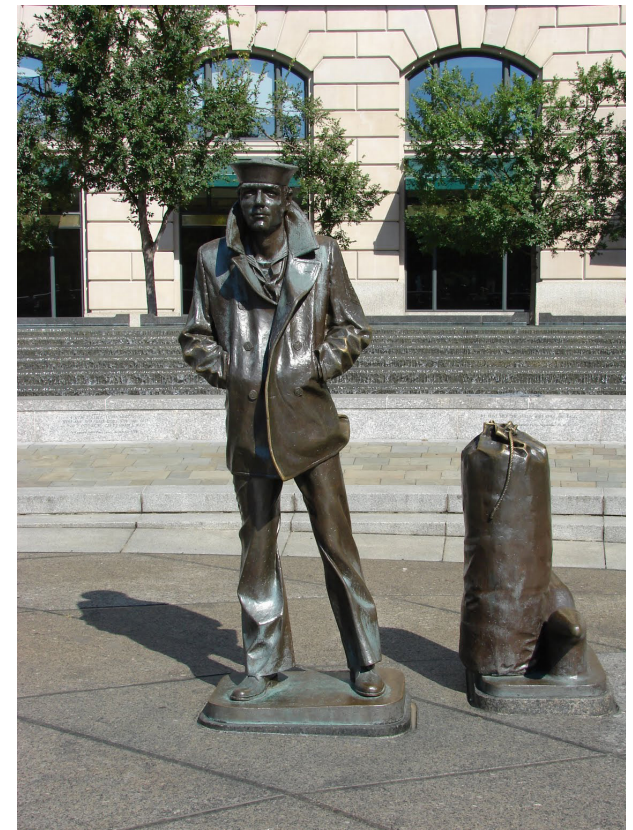
Triangle Park, located at 12th and Independence presents an opportunity for a civic plaza and commemorative elements at the terminus of Virginia Avenue.

- › Site design should capitalize on its relationship to the National Mall along the 12th Street axis and views of the Washington Monument.

- › The design of the building to the south (parcel 1c) should give form and shape to the plaza and support activity within the plaza.

Below: The Navy Memorial frames a key view north along 8th Street to the National Portrait Gallery. The design of this memorial should be used as inspiration for the future Triangle Park at 12th and Independence.

Right: Lone Sailor Statue at the Navy Memorial





For more information on the SW Ecodistrict project, please visit www.ncpc.gov/swecodistrict

